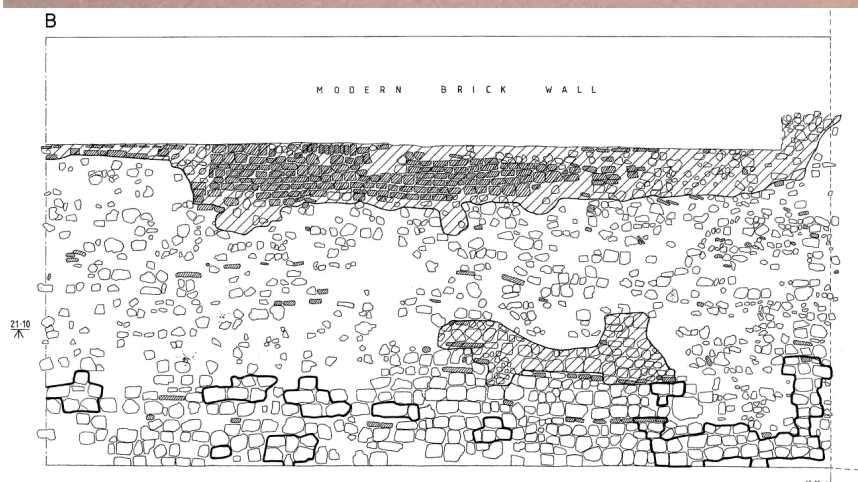


A survey of the Town Wall in the Bus Depot, Queen Street, Colchester, Essex

On behalf of Colchester Borough Council



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CAT project ref.: 16/02h

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1 Summary

This is a survey of the 33-metre long section of upstanding Town Wall, which now forms the southern boundary of the eastern half of the (now disused) Bus Depot, Queen Street, Colchester. The exposed section of wall is post-medieval in date, but it is possible (as seen elsewhere on the wall) that the original core survives behind and below the exposed face.



Plates 1a, b: View east showing the town wall, behind the steel structure of the Bus Depot. (The skylights are not helpful for photography).



Plates 1c, d: View west showing the town wall, behind the steel structure of the Bus Depot. (The skylights are not helpful for photography).

The archaeological and historical background of the Priory Street Car Park and the Bus Depot has already been comprehensively explored in:

A desk-based assessment of the archaeological remains at Priory Street Car Park and the old Bus Depot, Colchester, Essex: February 2016 (CAT Report 903).¹

The wall surveyed and reported on here is the reverse side of the town wall now visible in Priory Street, and coincides with [Plate 2](#), below. Oblique views of the outer face are given as [Plates 2b-d](#), below.



Plate 2a: View north of the outer face of the town wall in Priory Street (the reverse side of the Bus Depot wall). Showing the brickwork and skylights on top of the wall.



Plate 2b: oblique view east of south face of wall

¹

This should be referred to for the wider archaeological and historical background.



Plate 2c: oblique view east of south face of wall, including Bastion 5



Plate 2d: oblique view west of south face of wall

2 Aim

- 2.1 The aim of this survey is to provide a detailed drawing (with accompanying photographs) of the internal elevation of the Roman and later town wall, as exposed in the Bus Depot. There is an historic background of the wall's archaeological setting (Section 3), a text description with line drawings, and a discussion of dating (Sections 4, 5). Greater detail of the wall structure is given in Appendices, as follows:

Appendix 1 - the wall split into two-bay sections

Appendix 1 - drawing of whole wall length with general photographs

Appendix 3 - a reproduction of the CAT 1988 wall survey with 1988 B/W photographs

Appendix 4 - the 1987 survey with 2016 colour photographs



Plate 3: View west of the western end of the surviving town wall, showing the pipes and bus depot paraphernalia.

2.2 Difficult working conditions

This was a very difficult job, because the wall is obscured by huge screens, lockers, pipework, steel frames, and other impediments associated with the Bus Depot. Environmental hazards include a covering of pigeon droppings and a covering of soot from bus exhausts.

None of these conditions make for an ideal survey. There is no question that a better survey could be done if the site were cleaned up (netting removed, pigeon droppings cleared, exhaust deposits cleaned off wall, leaves swept up) and all encumbrances removed (lockers, the two huge screens, and redundant pipework).

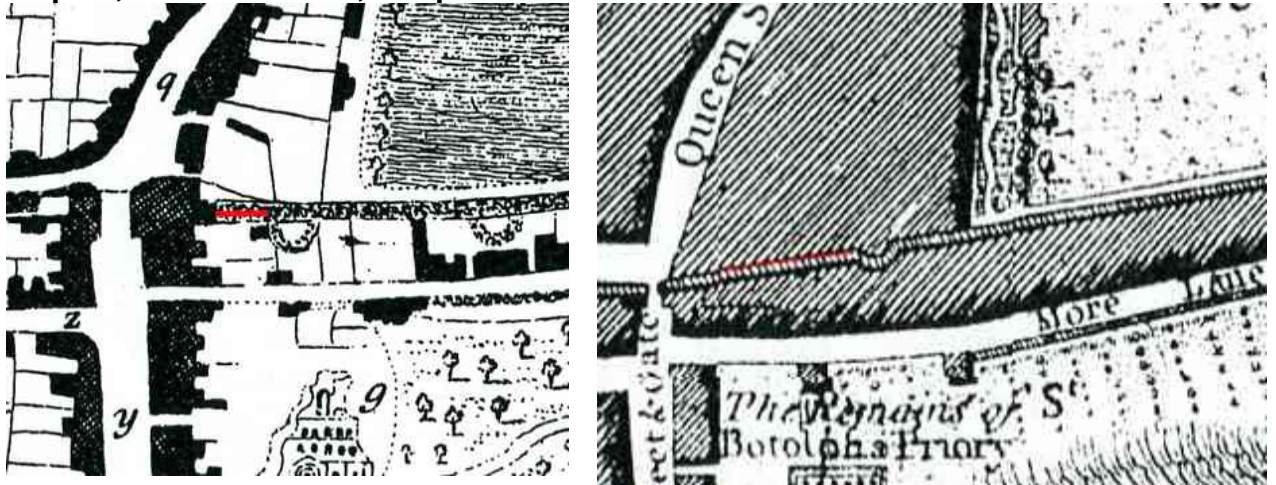
It was the intention to use rectified photographs as the basis of an elevation drawing created in CAD. However, this proved difficult to deliver because the pipes and steel frames prevented a direct view of the wall face, and (at the eastern end), screens and lockers hide most of the wall. Also, the skylight over the top of the wall floods light into all photographs. Nevertheless, these difficulties have been worked around as far as possible, and this report presents the best results which can be obtained in the present circumstances and within the budget available.

- 2.3 The wall is a Scheduled Ancient Monument (Historic England ref: 1003772). HE advised that no Scheduled Monument Consent was needed for this survey, since it is non-intrusive.

3 History of the Roman wall, as shown on maps

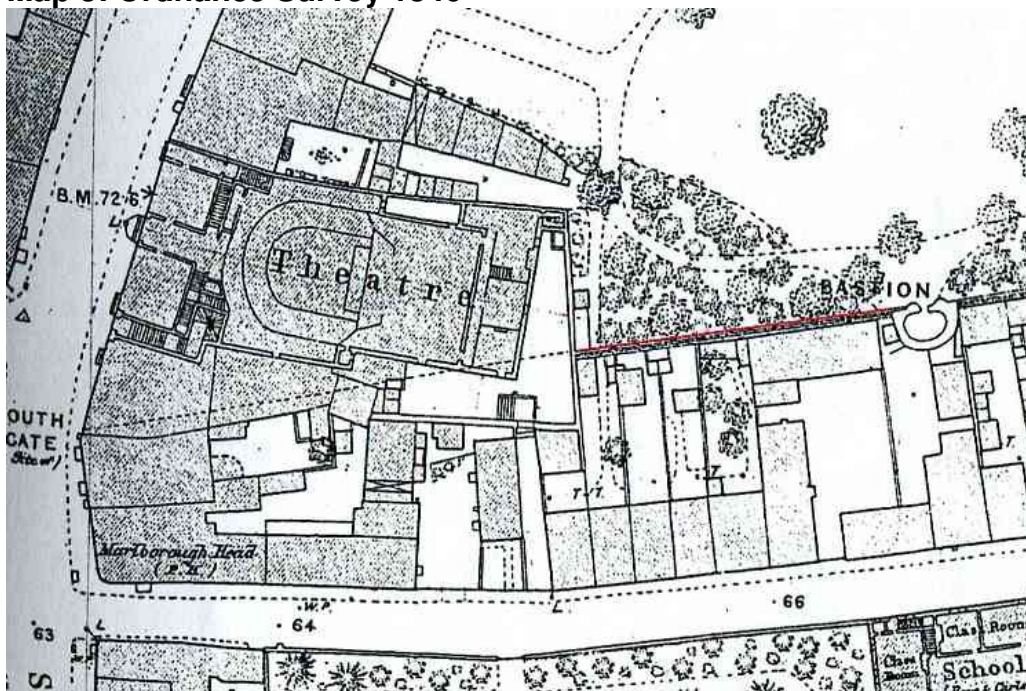
This section gives a selection of historic maps showing the position of the surveyed wall, as closely as possible (given varying scales of original maps) from Philip Morant (1748) to the Ordnance Survey of 1953, with appropriate commentary.

Maps 1, 2: Morant 1748, Chapman and André 1777



These are the earliest available maps. The surveyed wall is shown as a red line (as on all these figures). Too small a scale for sensible comments, except that in 1748 South Gate is still visible, straddling Queen Street (keyed 'q'). Bastions 5 and 4 are shown. In 1777, Priory Street was 'More Lane', and Bastion 5 is shown.

Map 3: Ordnance Survey 1846

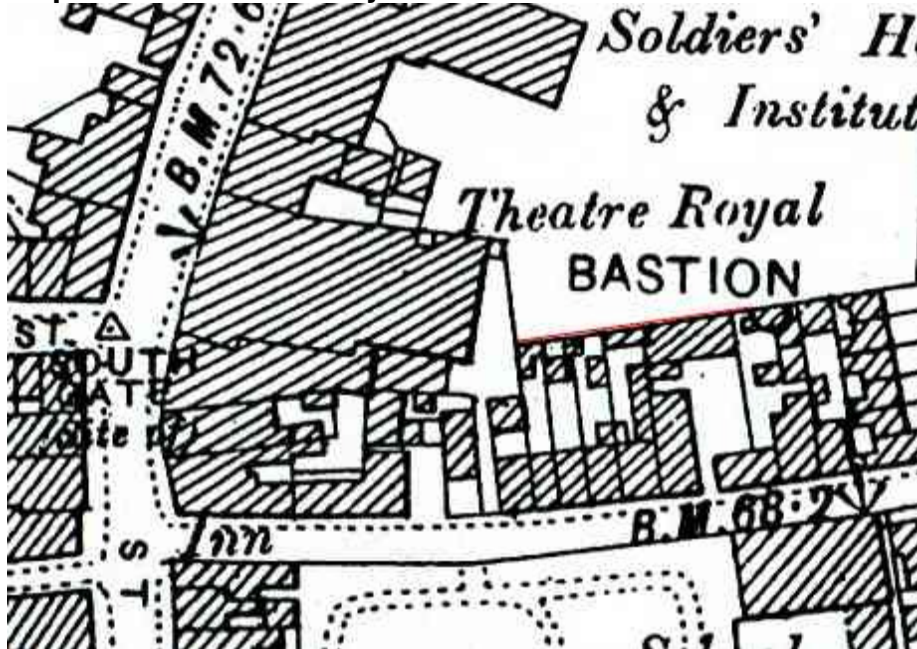


This map gives us the first good detail. Priory Street is entirely built-up, with some open gardens and structures built against the face of the wall. Bastion 5 is shown, accessed through the large garden (now occupied by the eastern half of the Bus Depot).

It is interesting that although the **Theatre Royal** had broken south through the line of the Roman wall, the wall line is still indicated on this map. There is some detail of

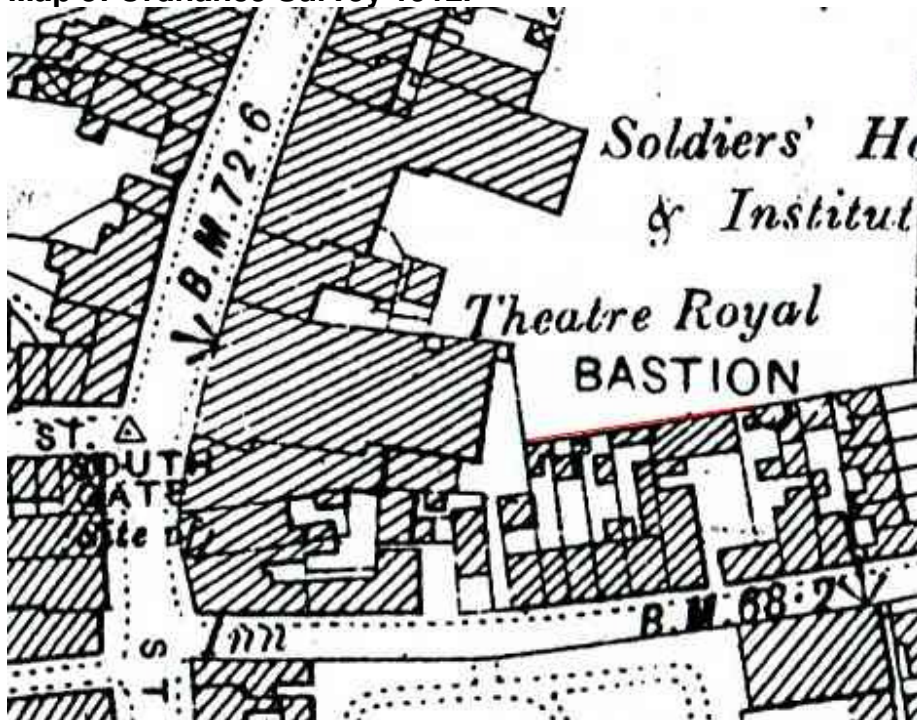
interest to the current survey. The banked theatre seating is at the Queen Street end, and the stage to the rear. There are storage rooms rear of the stage, to which access would be required, and to the rear of the theatre is what looks like an open yard (for scene painting, etc). The line of the Roman wall is dotted through this space, so it looks like the wall had been reduced to below the level of the theatre yard (or else demolished).

Map 4: Ordnance Survey 1897



Broadly the same as 1846, except some of the theatre yard roofed over?

Map 5: Ordnance Survey 1912.



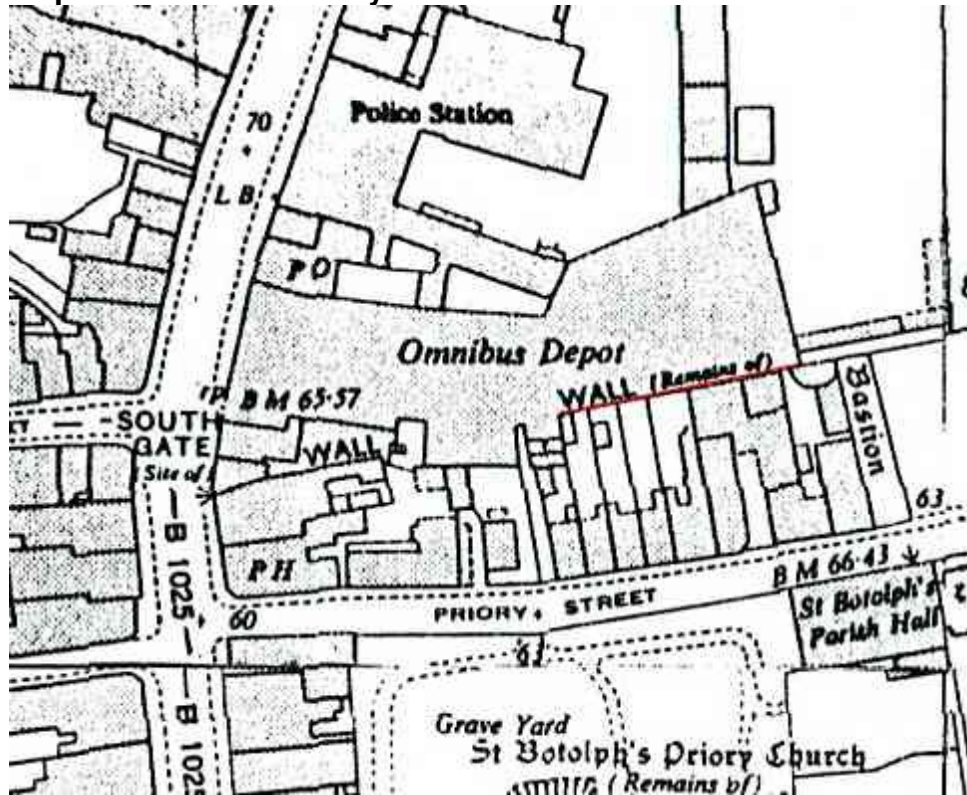
No significant changes since 1897.

Map 6: OS 1923



What was an open yard rear of the theatre in 1846, and partially roofed over in 1897 is now shown as completely built over, presumably with theatre-related structures.

Map 7: Ordnance Survey 1953



The Bus Depot has taken on its current ground plan, with its eastern half now occupying land which was open garden from 1748, and the grounds of the Soldiers Home in 1923.

4 A description of the wall

4.1 The basic structure

It is helpful to first look at a photographs of the other side of this wall, as seen in Priory Street car park (Plates 4-6). This will give us a way in to the discussion and understanding of the Bus Depot wall. The Priory Street wall consists of two elements: first, the rubble and tile/brick outer face: second, on top of the older wall, courses of post-medieval bricks which support the frame of a row of skylights. One point which is not obvious from Plates 3-5 is that the post-medieval brick courses are not laid uniformly over the older rubble wall. At the western end of the wall (plate 4), the brick wall is set back by 0.4-0.5m from the rubble wall face, whereas at the eastern end of the wall (plate 6), the brick wall is more nearly flush with the rubble wall face.



Plates 4-6. View north, from west to east, of the outer face of the town wall (2016)

4.2 The post-medieval elements

The upper brick courses (seven courses of Flemish Bond topped by a row of headers), look like they fit well with the skylight superstructure, and it may be inferred that they are contemporary with it. Maps (see above [Section 3](#)) show that the Bus Depot was in place by 1953, but was not there in 1923. Rex Hull, then Museum Curator, records that the bus company was creating a car park in 1931 on the land east of the present Bus Depot. It may be that the Bus Depot dates from that time. If this is so, then the upper eight courses of brickwork must be of this date, although they effect an earlier style and appearance.

The lower courses of brick (eight, including a tile string course, topped by a row of headers), have a different feel. They are slightly darker in colour, and a mixture of Flemish Bond and a combination of headers and stretchers. In all probability, they are an earlier structure, clearly post-medieval on account of the bricks (which are closer to the regular 2-inch-thick 'Tudor' than the modern 2.5-inch). One clue may be the fact that the town wall east of the bus depot and its parking area is also topped by a post-medieval brick wall. It can be seen on the maps ([Section 3](#)) that there was a large garden on the Bus Depot site as far back as the Morant and Chapman & Andre maps of 1748 and 1777. It may be the case that the Bus Depot lower brick wall is contemporary with that garden, and that when the Bus Depot was built in the mid-20th century, the upper brick courses supporting the roof were added on top of an existing brick wall.

4.3 The wall as it is exposed in the Bus depot

Larger-scale versions of the figures given here are in [Appendices 1-4](#). The exposed town wall is approximately 33.4m east to west, and forms the lower part of the southern boundary of the eastern half of the Bus Depot. The most easterly surviving point on the wall is 42m east of the sliding doors on the Bus Depot Queen Street entrance. (Over that 42m length, the Roman wall is invisible, because it is below the floor of the western half of the depot).

In contrast to the view from the Priory Street side ([4.1 above](#)), there are not two but three visible elements to the wall.

(1) The lowest and widest element appears to be a post-Roman rebuild of the original town wall. Its top is at 22.38m at the west end, and 22.36m at the east end. How do we date a wall? For the Colchester wall, the survival of the regular pattern of four courses of septaria topped by four courses of brick, such as can be seen around the Balcerne Gate, is a good sign that the original Roman face survives intact. In fact, no such facing is to be seen here. The conclusion must be that this is a post-medieval rebuild of the Roman wall.²

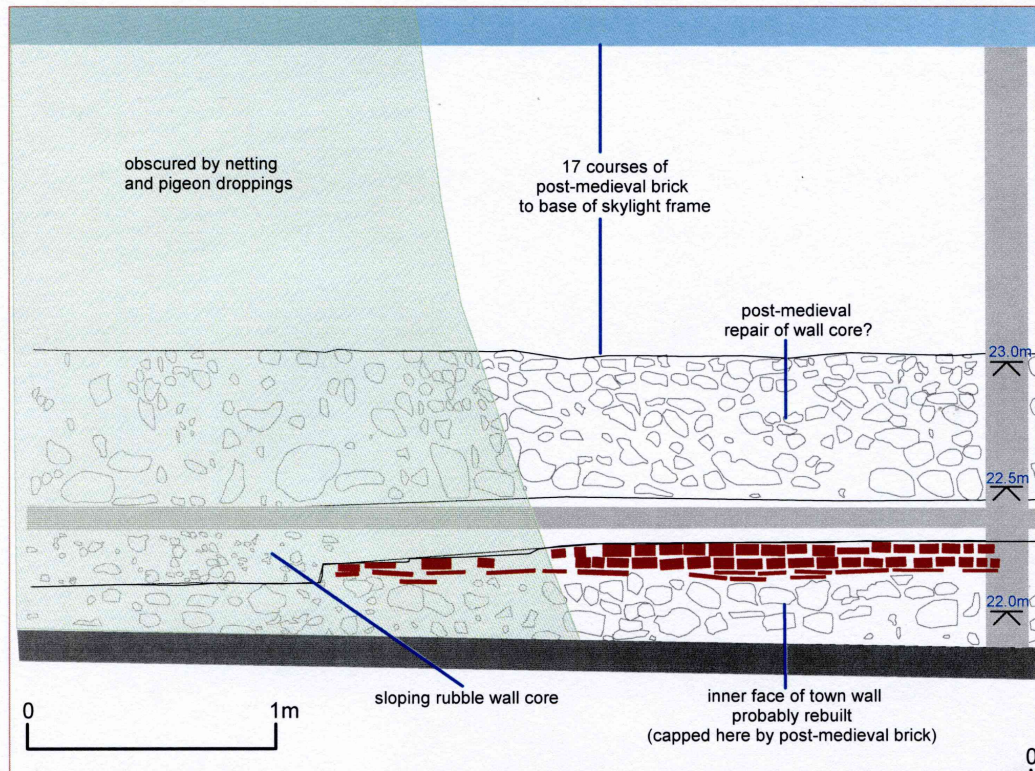
(2) On top of the core, there is a much narrower (30cm) wall consisting of septaria rubble. This is generally 0.55 - 0.6m high over the wall core, and its top is generally at 23.05m along the eastern end of the wall, and 22.8m along the western end. It is set back by between 0.8m and 0.6m from the inner wall face.

(3) On top of the narrow rubble wall is a post-medieval brick wall, of between 15 and 22 courses, up to the underside of the frame holding the skylights. There is a course of headers half-way up this wall, suggesting two periods of structure.

² However, the inner face, where it survives, is neatly constructed. This may indicate that it is on the line of the original Roman structure

4.4 Description of the wall as it is exposed in the Bus depot (Bays 1-7)

Bay 1



Description:

Bay 1 is at the east end of the Bus Depot. The eastern end of the Bay 1 wall face survives badly, standing only 0.3m high³. The western end survives 0.43m high, the top 0.13m being a capping of one or two courses of peg-tile, supporting two courses of post-medieval brick ('Tudors'). The width of the wall could not be measured here, but is 1.5m on the Survey Solutions (SS) survey (31/07/14 - drawing number 14543se-01).

To the rear of the inner face, the wall core slopes up gently to meet a near-vertical face of mortared septaria (also visible from Priory Street), 0.89m rear of the inner face, and standing 0.6m high above the wall core. This is 0.3m wide on the SS survey.

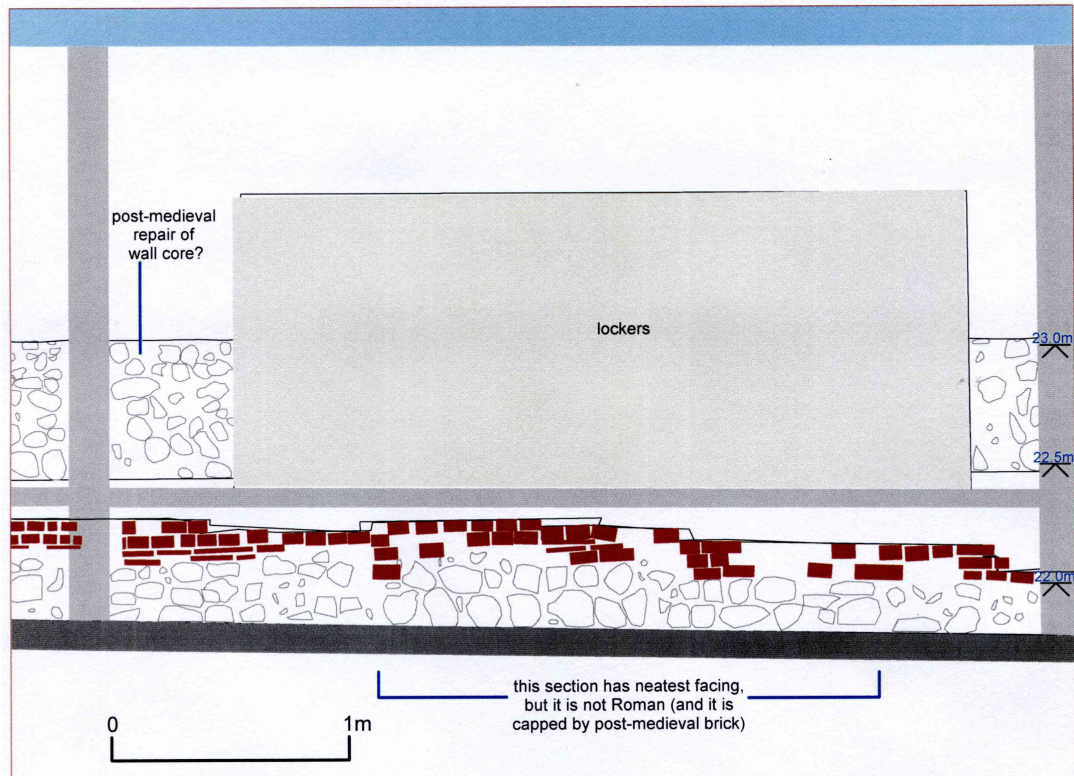
On top of the rubble wall inner face are 17 courses of post-medieval brick, split into two parts by a row of headers half-way up. The top 7 courses and the capping header row fit so neatly under the wooden skylight frame that they must be contemporary with the construction of the Bus Depot (it would be too much of a coincidence that there was a convenient brick wall of the correct height already there when the Bus Depot was built). However, the lower headers and the 9 courses below may well be an earlier build.

Comment:

The wall core appears to have been trimmed down to the height of the Bus Depot steel frame along most of the exposed length. Particularly difficult to survey here due to obstacles (netting, pipes, steel frame, leaves and debris, pigeon droppings).

³ above the concrete plinth alongside the wall, generally 13cm above the depot concrete floor

Bay 2



Description:

Bay 2 is west of Bay 1, and east of Bay 3. The eastern end of the Bay 2 wall face stands 0.43m high, the top 0.13m being a capping of one or two courses of peg-tile, supporting two courses of post-medieval brick ('Tudors'). The western end is slightly more damaged, and stands only 0.25m high. It is capped by one course of Tudors (3 courses just to the east).

The wall width could not be measured here, but is 1.5m wide on the SS survey.

There is a length of the inner face in Bay 2 (a little over 2m long - above) which is more regularly faced than elsewhere at the Bus Depot. Is this original Roman wall facing (plate 8). On balance, probably not - it is not quite neat enough, and against this is the fact that it is capped by post-medieval brick (no Roman brick courses are exposed in the Bus Depot, though the hidden wall core is probably Roman or medieval).



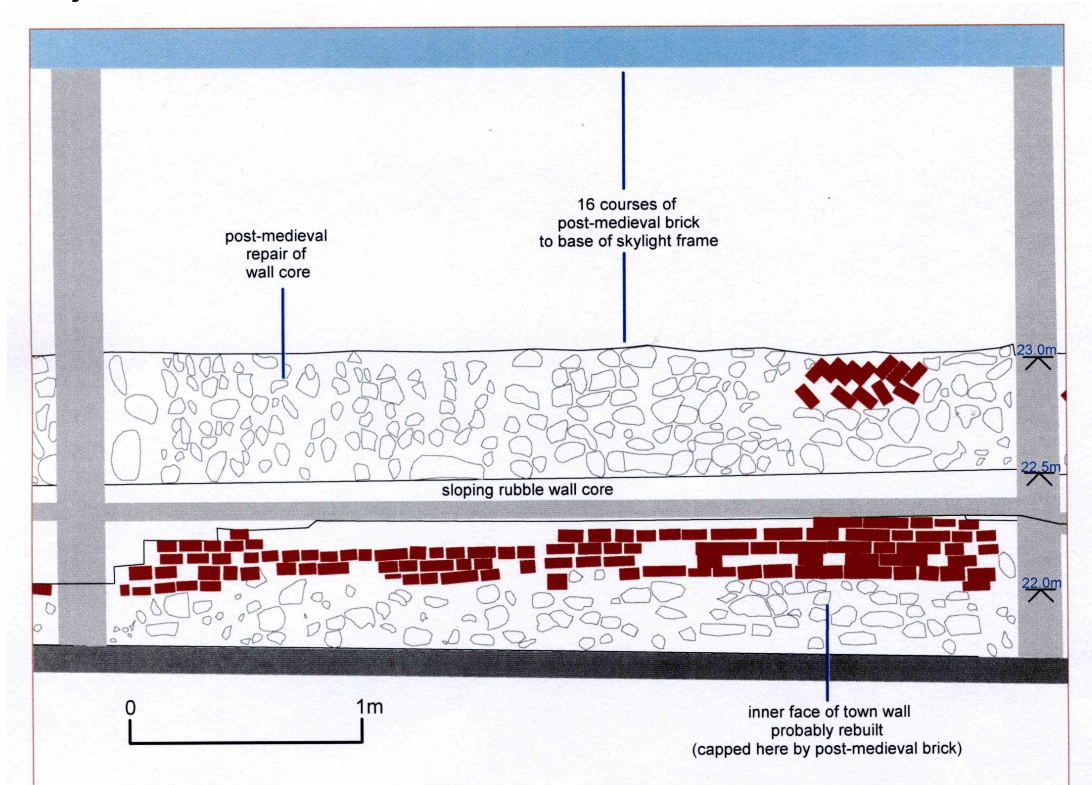
Plate 8: under lockers in Bay 2 the septaria is more regularly-coursed than elsewhere in depot, but this is not Roman-period.

To the rear of the inner face, the wall core slopes up gently to meet a near-vertical face of mortared septaria (also visible from Priory Street), 0.83m rear of the inner face⁴, and standing 0.6m high above the wall core. It was not possible to measure the width of this face, but the Survey Solutions survey indicates it is 30cm wide.

Comment:

The wall core appears to have been trimmed down to the height of the Bus Depot steel frame. The majority of this bay is blocked by lockers. Other obstacles: pipes, steel frame.

Bay 3



Description:

The extreme eastern end of the wall face stands only 0.25m high, and is capped there by a single course of post-medieval brick. However, the majority of the wall face in Bay 3 stands 0.35m high above the concrete plinth, and is capped by up to five courses of post-medieval brick (0.35-38m high). There are no courses of peg-tile here.

The wall width could not be measured here, but is scaled off the SS survey as 1.5m.

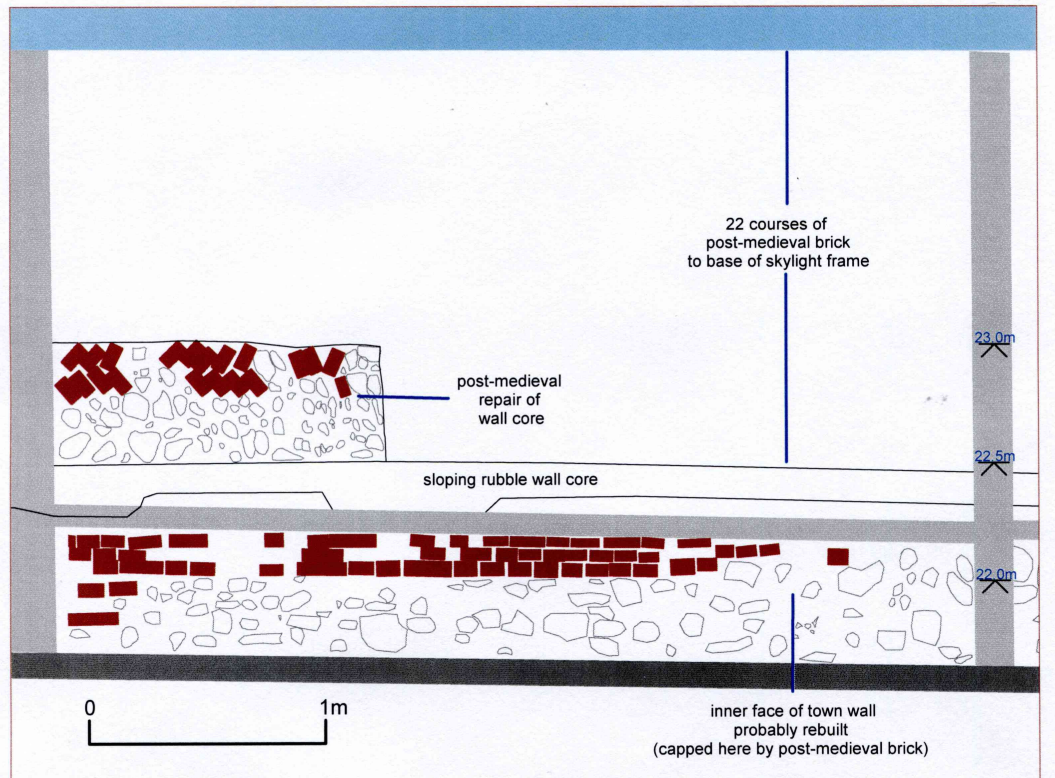
To the rear of the inner face, the wall core slopes up gently to meet a near-vertical face of mortared septaria (also visible from Priory Street), 0.77m rear of the inner face, and standing 0.6m high above the wall core. It was not possible to measure the width of this face, but the SS survey indicates it is 0.3m wide. This wall face here includes a group of Tudors in a rough herringbone pattern. These bricks help to date the repair/consolidation of this part of the wall face.

Comment:

Direct photographs hampered by a large screen. Other obstacles: pipes, steel frame.

⁴ this is a smaller distance than in Bay 1

Bay 4



Description:

The inner wall face survived higher here than in Bays 1-3 to the east - 0.7m above the concrete plinth, including up to five courses of post-medieval brick. At the western end of this bay, there is no brick capping, only a rubble wall face standing 0.5m above the concrete plinth. There are no courses of peg-tile here.

The width of the wall could not be measured here, but is 1.5m on the SS survey.

To the rear of the inner face, the wall core slopes up gently to meet a near-vertical face of mortared septaria (also visible from Priory Street), 0.69m rear of the inner face, and standing 0.6m high above the wall core. It was not possible to measure the width of this face, but the SS survey indicates it is 30cm wide. As in Bay 3 to the east, the wall face here includes a group of Tudors in a rough herringbone pattern. These bricks help to date a phase of repair/consolidation of the wall including this inner wall face.

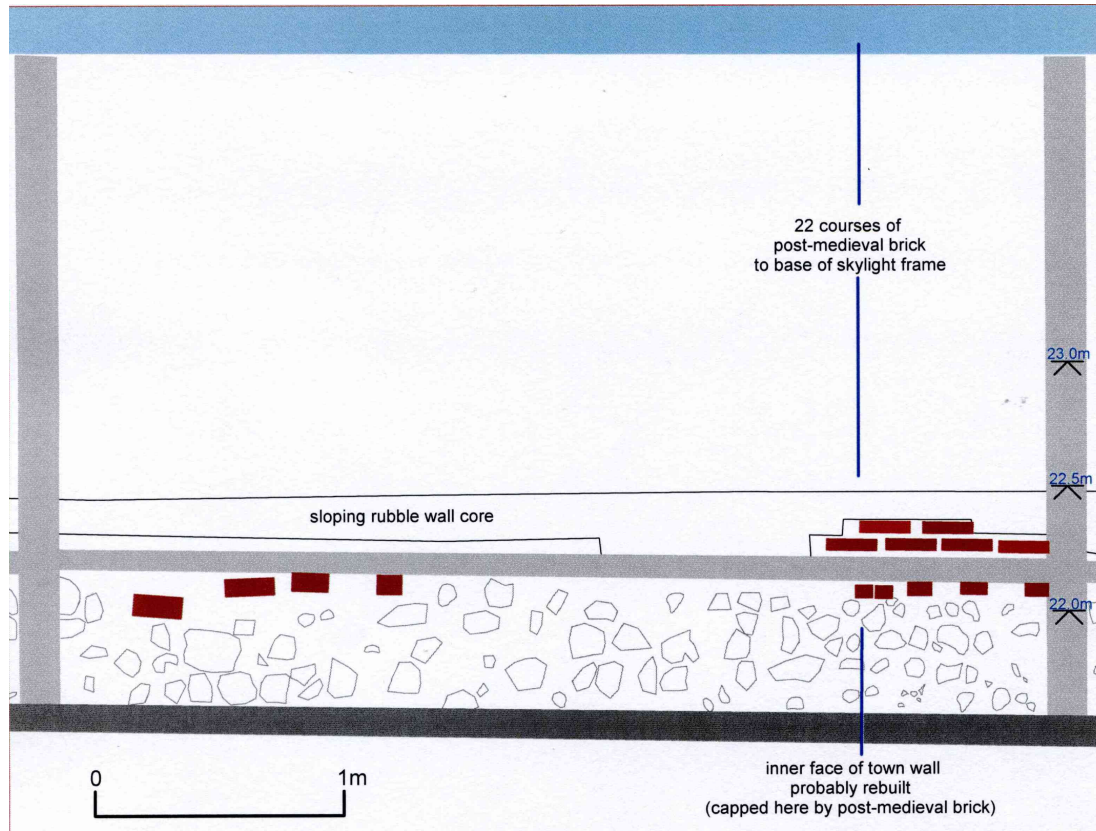
There is a major difference here in that the inner wall face just discussed comes to an end in this bay (it resumes in Bay 6), and its place is taken by a greater height of post-medieval brickwork than in bays to the east (22 here, as opposed to 16 or 17 to the east).

Comment:

The wall core appears to have been trimmed down to the height of the Bus Depot steel frame.

Obstacles: pipes, steel frame.

Bay 5



Description:

Bay 5 is the only one in which there is no upper rubble wall face, the post-medieval brickwork resting directly on the wall core. The inner wall face survives as high here as in Bay 4 to the east - 0.7m above the concrete plinth, including up to five courses of post-medieval brick at the western end. There are no courses of peg-tile here. The septaria is quite regularly-coursed at the east end, but not so regular as to be potentially Roman.

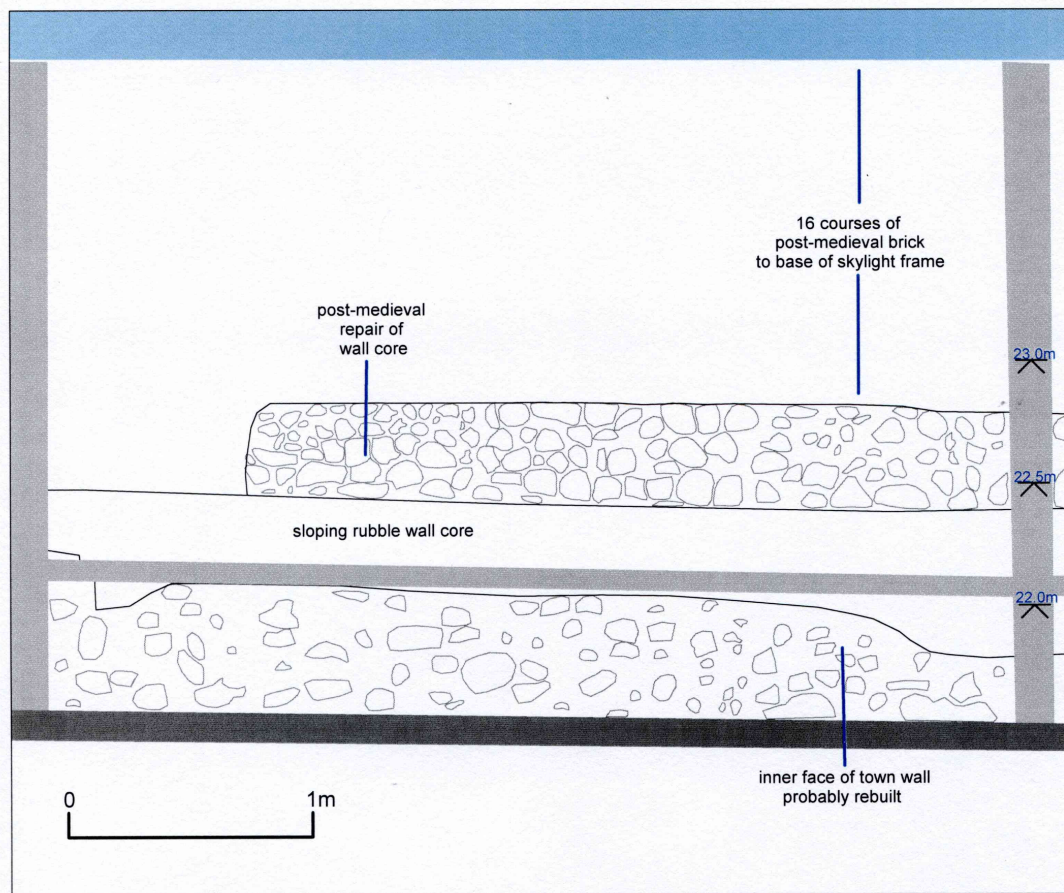
The width of the wall could not be measured here, but is given as 1.5m on the SS survey.

Comment:

The wall core appears to have been trimmed down to the height of the Bus Depot steel frame, and tidied up with extra post-medieval brickwork. The wall face is heavily plastered over, making it difficult to see individual septaria stones.

Obstacles: pipes, steel frame.

Bay 6



Description:

There is no brickwork in the inner wall face in this bay. The inner wall face (rubble only) has been severely hacked here, and survives to varying heights from 0.25m to 0.65m. It is noticeable how exposed the rubble wall core is here, and that it slopes more than in any other bay (i.e., there is a difference of between 0.35m and 0.60m in height between the top of the inner face and the bottom of the rear rubble face).

The width of the wall could not be measured here, but is given as 1.5m on the SS survey.

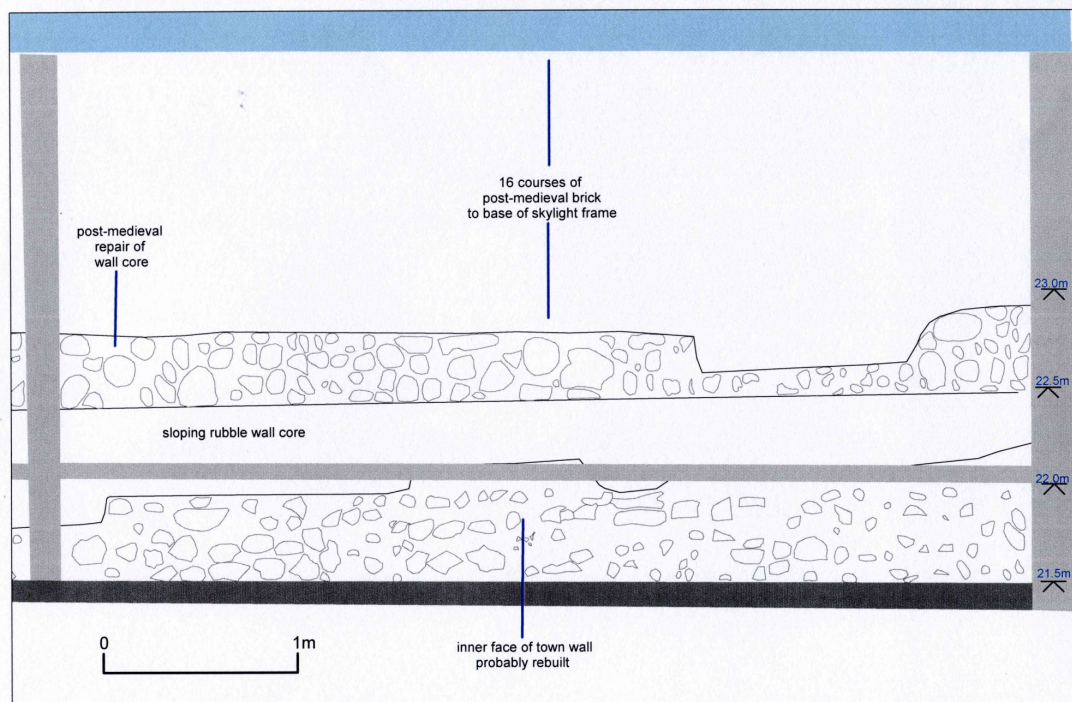
To the rear of the inner face, the wall core slopes up gently to meet a near-vertical face of mortared septaria (absent in Bay 5 to the east), 0.66m rear of the inner face, and standing 0.40m high above the wall core. It was not possible to measure the width of this face, but the SS survey indicates it is 30cm wide. Unlike Bays 3 and 4, there are no Tudor bricks in this inner wall face.

Comment:

The wall core severely damaged here, and the wall face is heavily plastered over, making it difficult to see individual septaria stones.

Obstacles: pipes, steel frame.

Bay 7



Description:

There is no brickwork in the inner wall face in this bay. As in Bay 6, the inner wall face (rubble only) has been severely hacked here, and survives to varying heights from 0.25m to 0.65m. As in Bay 6, it is noticeable how exposed the rubble wall core is here, and that it slopes more than in any other bay (i.e., there is a difference of between 0.35m and 0.70m in height between the top of the inner face and the bottom of the rear rubble face).

The width of the wall could not be measured here, but is given as 1.5m on the SS survey.

To the rear of the inner face, the wall core slopes up gently to meet a near-vertical face of mortared septaria, 0.54m rear of the inner face, and standing 0.5m high above the wall core⁵. It was not possible to measure the width of this face, but the SS survey indicates it is 30cm wide.

Comment:

The wall core severely damaged here. The wall face is heavily plastered over, making it difficult to see individual septaria stones.

Obstacles: pipes, steel frame.

⁵ the inner wall face and its brick topping are much nearer to the inner face in Bay 7 (0.54m), than in Bay 1 (0.89m).

5 Archaeological summary of Bus Depot and the wall

Archaeological context of Bus Depot ⁶

The Bus Depot is in insula 38b of the Roman town (Crummy 1992, 15). There have been no archaeological excavations inside the Bus Depot. The nearest is the trench dug by Rex Hull in 1931, in what was then the yard rear (east) of the Bus Garage. Here he found Roman wall lines and a drain. Other local discoveries of Roman walls and floors within 20m of the Depot reinforce the impression that insula 38b was built-up with Roman town houses, in the manner demonstrated by large-scale excavations at Lion Walk and Culver Street, to the west (Crummy 1984, Crummy 1992). In fact, the Bus Depot straddles a Roman gravel street, and so there are probably Roman buildings on both the north and south sides of this street. Figure 1 of the Desk-based assessment is Appendix 5 of this report).

The only known and significant remains post-dating the Roman town-houses are the foundations of the Theatre Royal, which occupied this site before the Bus Depot. It opened in 1802, reopened after a refit in 1889, and closed after a disastrous fire in 1918 ⁷. Such luminaries as Edward Terry, Gladys Cooper and Mrs Patrick Campbell trod the boards there.

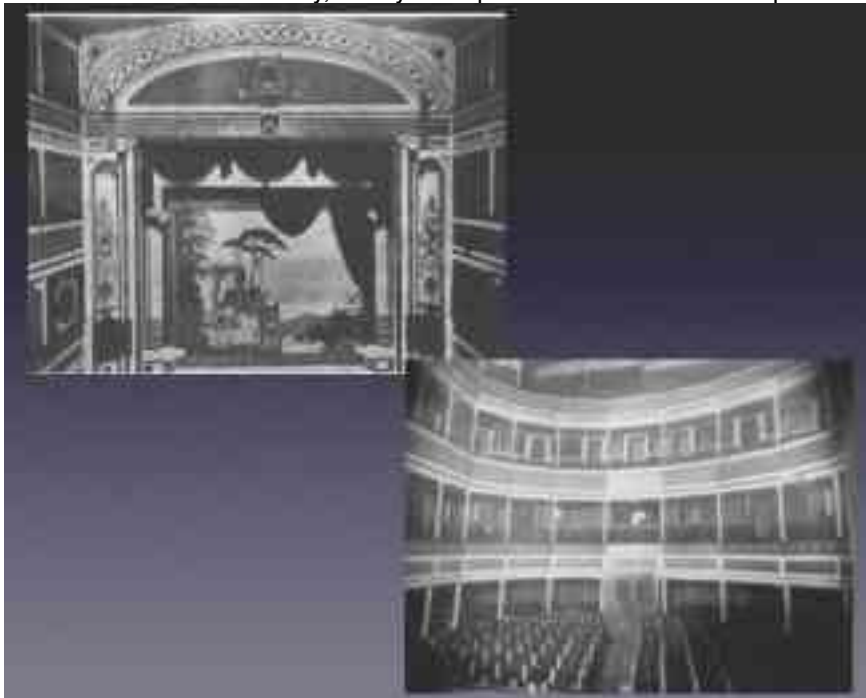


Plate 9: of the interior of the Theatre Royal, Colchester⁸

It is not known whether walls and foundations of the theatre survive, but it is more than a possibility. Likewise, it is the experience of recent large-scale excavations in the town that although modern infrastructure (such as the Bus Depot) undoubtedly has a detrimental effect on the buried remains, it does not always destroy them completely - islands of preserved Roman deposits can survive between modern wall lines. Such may be the case with the Bus Depot.

⁶ for greater detail, see the Desk-based assessment of Priory Street Car park - CAT Report 903, February 2016

⁷ <http://www.arthurlloyd.co.uk/ColchesterTheatres.htm>

⁸ <https://colchesteruncovered.files.wordpress.com/2014/06/theatre-royal.jpg>

The wall structure

It is interesting that the view from Priory Street suggests a two-part structure - brickwork over rubble wall - whereas in the Bus Depot it appears as a three-part structure - wall core, inner rubble face, and brickwork (Plate 10). It seems that the view from inside is misleading - the wall core has been hacked back in the Bus Depot, leaving only a thin rubble wall (0.3m wide) standing over a more solid core.



Plate 10: views of wall from Priory Street, and inside Bus Depot

It can also be seen from Priory Street that the brickwork is in two distinct colours (Plate 10). This probably indicates two distinct structural phases - later one being the upper seven courses supporting the Bus Depot roof, and the earlier one being the lower seven courses (or more) on top of the Roman wall, each phase being topped by a course of headers. Some support for this view is shown in Plate 11, where it can be seen that to the south of the old Bus Park (now VAF), the Roman wall is similarly topped by a post-medieval brick wall (seven courses plus headers). It can therefore be concluded that the brick wall on top of the Bus Depot wall is indeed of two periods - an earlier (post-medieval or Victorian) wall on top of the Roman wall, and a mid-20th-century wall built to support the Bus Depot roof.



Plate 11: long view of Bus Depot wall (left) and south of old Bus Park (right)

Width, date, height of wall

Width and date of wall

Elsewhere in the Roman town, the wall, when intact, is usually 2.6m wide (± 0.1 m; Crummy 2003). The wall width could not be measured in this survey, but it can be scaled off the 2014 Survey Solutions drawing number 14543se-01 as 1.5m. Contrary to Philip Crummy's figure of 2.6m, Rex Hull saw the wall immediately east of the Bus Depot (in the parking area) where he measured it at 4 feet 6 inches (1.37m). Judging by Philip Crummy's figure of 2.6m (\pm), at least 1m is missing off the Bus Depot wall width here, and it seems to be closer the 4'6" width seen by Rex Hull in 1931.

There is comparative evidence from a 1990 evaluation connected with an earlier plan to redevelop the Bus Park (CAT 1990: see Plates 12, 13 below). A five-trench evaluation included two against the inner face of the town wall. The wall was measured at 2.8m-wide in Trench 2 (33m east of the Bus Depot) and at 2.5m wide in Trench 3, 78m east of the Bus Depot. These differing wall widths can be accounted for by repair, rebuilding etc, and where the wall is not at Crummy's official 2.6m, we can assume that some of the face has been quarried away. It is recorded elsewhere that severe damage was caused to the wall during the Siege of Colchester in 1648, and that large parts of it were reconstructed thereafter.



Plate 12: Inner wall face exposed in 1990 evaluation Trench 2, 33m east of the east wall of the Bus Depot. Note the neat coursing of the in situ Roman wall face. The highest point on this surviving coursing is 21.25m (view south: scales are 1m long)



Plate 13: Inner wall face exposed in 1990 evaluation Trench 3, 78m east of the east wall of the Bus Depot. Note the bonding courses of Roman brick, heavily mortared here (view south: scale is 2m long)

As the Bus Depot wall is only 1.5m wide, it can be assumed that it is a post-medieval rebuild of the original Roman wall, but on a much smaller scale (and possibly on top of the original Roman foundation, which has not seen in recent times). The best context for such a major rebuild would be after the Civil War and the siege of 1648.

Rex Hull reports seeing about 4 feet of the inner face of the Roman wall when the bus company lowered ground level in the yard east of the bus depot. He says (1958, 51):

"All this part of the wall is of one continuous build, of small pieces of septaria and without any tile lacing courses, and thus totally different from any known part of the original Roman wall".

Of course, the piece of wall reported by Hull here is, beyond Bastion 5, a direct continuation of the face surveyed here inside the Bus Depot (Plates 14-17). We share Hull's view that the wall face is not original Roman, but a post-medieval rebuild (the Roman or medieval core may survive behind it).



Plate 14: the wall Hull saw in 1931 is at the south edge of the bus yard (view south here). The wall is heavily overgrown.



Plate 15: centre of yard (above). Some wall fabric visible behind vegetation.



Plate 16: wall fabric exposed at west end of bus yard. Detail of rear view of overgrown Bastion 5 in Plate 17, below.



Plate 17: eastern side of Bastion 5, interior is overgrown.

Height

The top of the main wall core in the Bus Depot is at 22.38m AOD at the western end and 22.36 at the eastern. At the base of the wall in Priory Street, the car park surface is at 19.79m at the western end and 19.62 at the eastern. Therefore there is at least 2.59m of standing wall below the Bus Depot floor at the western end of the exposed section, and 2.76m at the eastern end.

In this context, it can be noted that the top of the surviving original Roman wall facing exposed 33m to the east in Trench 2 of the 1990 evaluation was at 21.25m. This height, projected into the Bus Depot, is approximately 0.70m below the floor level at the east end of the Bus Depot (i.e., bay 1) and approximately 0.25m below the floor at the west end (Bay 7).

The hidden wall between Bus Depot and South Gate

The town wall extends west of the exposed section, under the floor of the Bus Depot, and must run past the South Gate (now demolished) on Queen Street.

The continuing line of the wall is shown on the 1846 OS map, even though the Theatre Royal had been built over it. The wall must have been exposed when the Theatre Royal was built, but there are no records. It may, of course, have been demolished. If it survives, it must be at least 2m high under the Bus Depot floor. It is not known whether the wall was seen when the Bus Depot was built - presumably so, but again there are no records.

However, the inner (i.e., northern) wall face was seen in 1997 during repairs inside number 48 Queen Street (now 'Kool Kutz' hairdressers). Here approximately 1.8m of wall elevation was visible in a 2m-wide strip. The report concludes that this piece of wall face had been reconstructed in the modern period, using a combination of post-medieval bricks and Roman rubble (Shimmin 1997). There is also a reference in Hull (1958, 58) to the wall being visible in the cellars of 'Messrs Wilson's shop', where there was an opening through it. By the context of the discussion, this should be on Priory Street, but Messrs Wilson's address is not known.

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7 Glossary

AS	Anglo-Saxon (AD 430 to 1066)
ECC	Essex County Council
EHHER	Essex Historic Environment Record, held by ECC
Iron Age	After the Bronze Age and before the Romans – 700 BC to AD 43
medieval	from AD 1066 to around AD 1530
modern	19th – 21st centuries
OS	Ordnance Survey
post-medieval	from 16th century to 18th century
prehistoric	before Roman period - i.e. the years BC to AD 43
Roman period	from AD 43 to around AD 410
SAM	Scheduled Ancient Monument
UAD	Urban Archaeological Database (Colchester Borough Council).

8 Acknowledgements

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Report researched and written by Howard Brooks, Colchester Archaeological Trust. April and May 2016.

Site survey by Mark Baister, Alec Wade, and Robin Mathieson. CAD plans by MB, RM, and HB.



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Appendices (following pages)

- Appendix 1 drawing of whole wall length with general photographs
- Appendix 2 the wall split into smaller sections
- Appendix 3 a reproduction of the CAT 1987 wall survey with 1987 B/W photographs
- Appendix 4 the 1987 survey with 2016 colour photographs
- Appendix 5 Figure 1 of the Desk-based assessment, showing local archaeological context



Fig 1 Bus Depot outline, with town wall visible in Bus Depot (dark green) and projected (pale green). The brickwork on top of the town wall is shown brown.

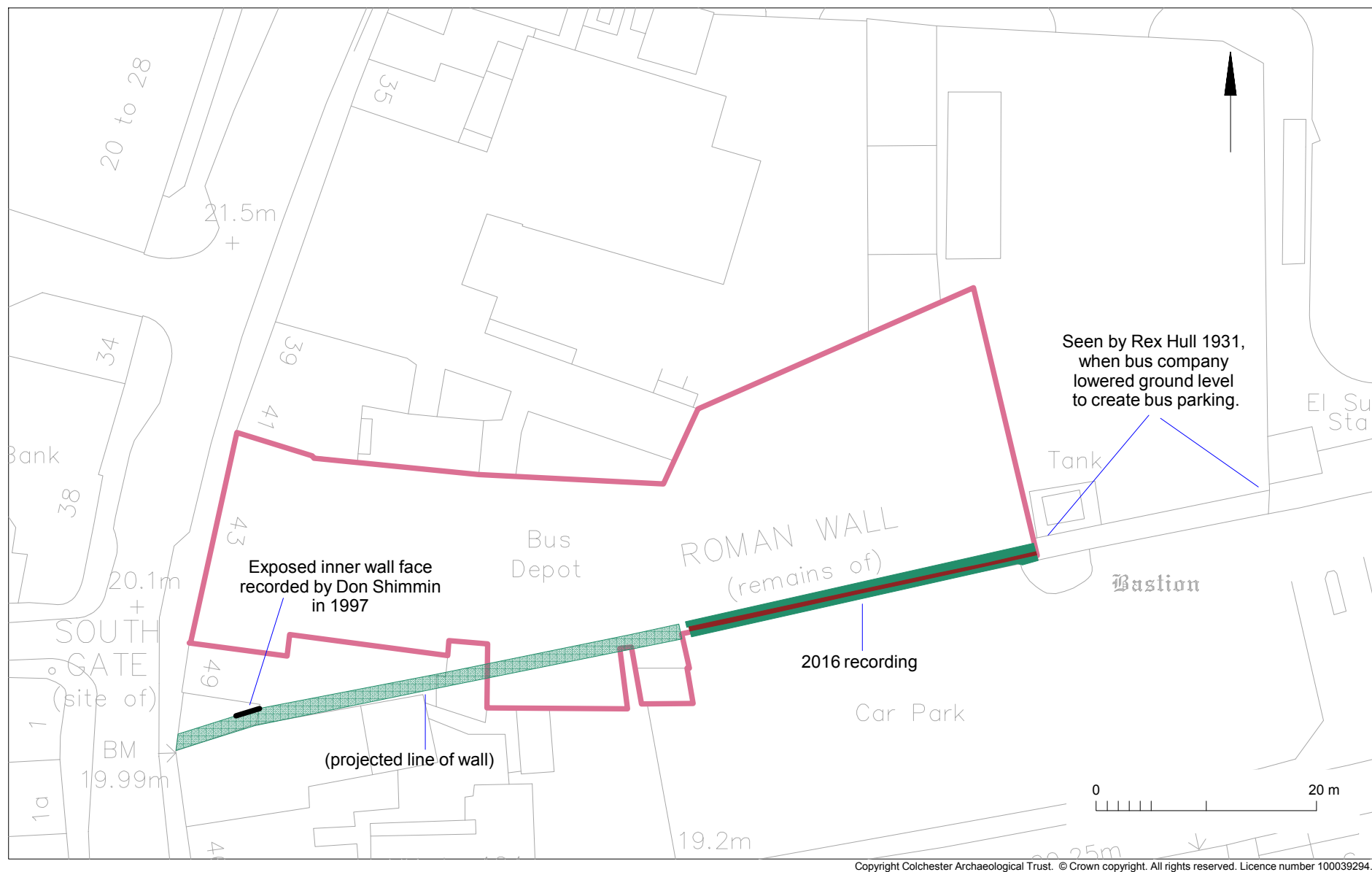


Fig 2 Bus Depot: comments on observations of wall structure.

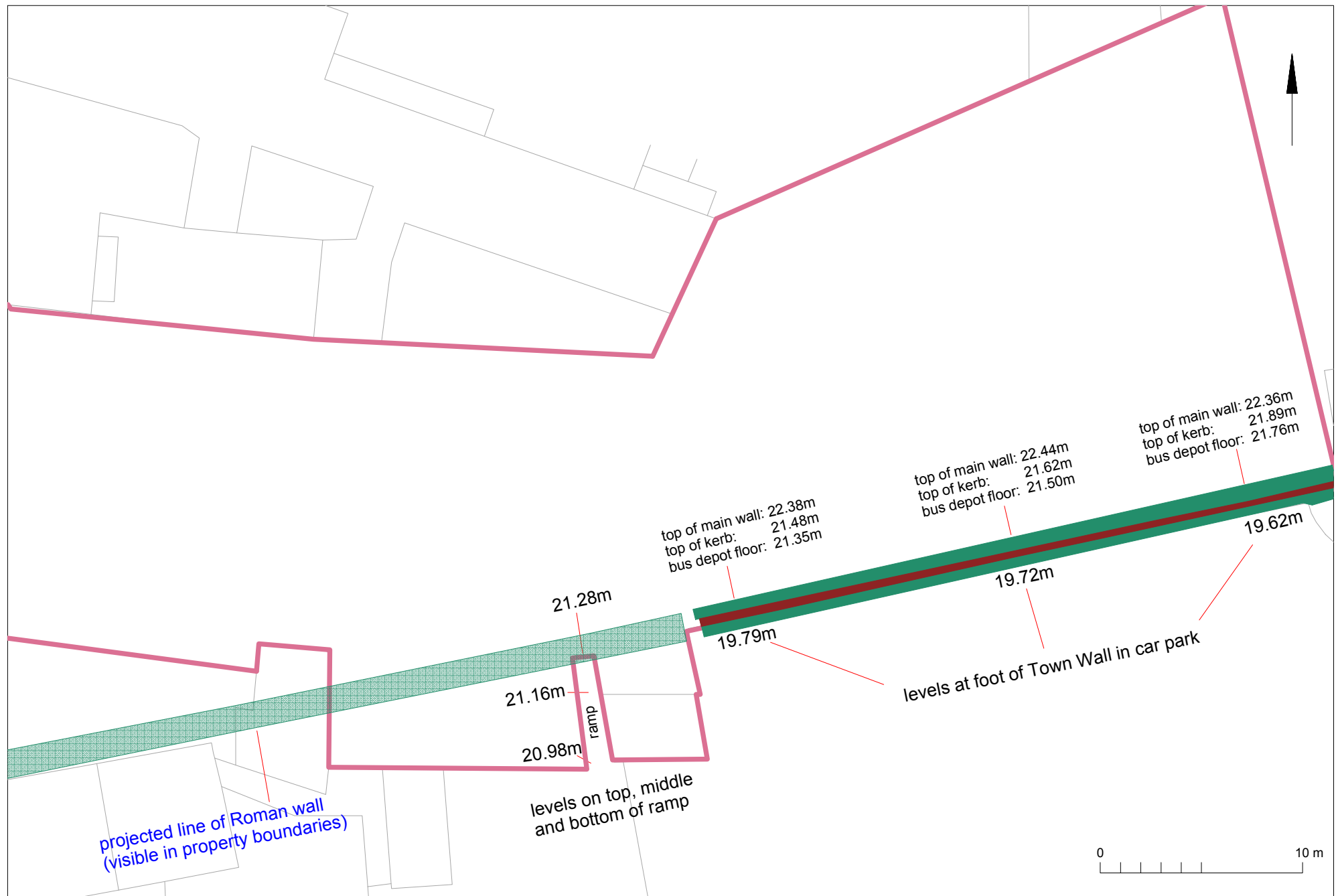


Fig 3 Levels on Town Wall in Bus Depot, in Car Park, and on ramp

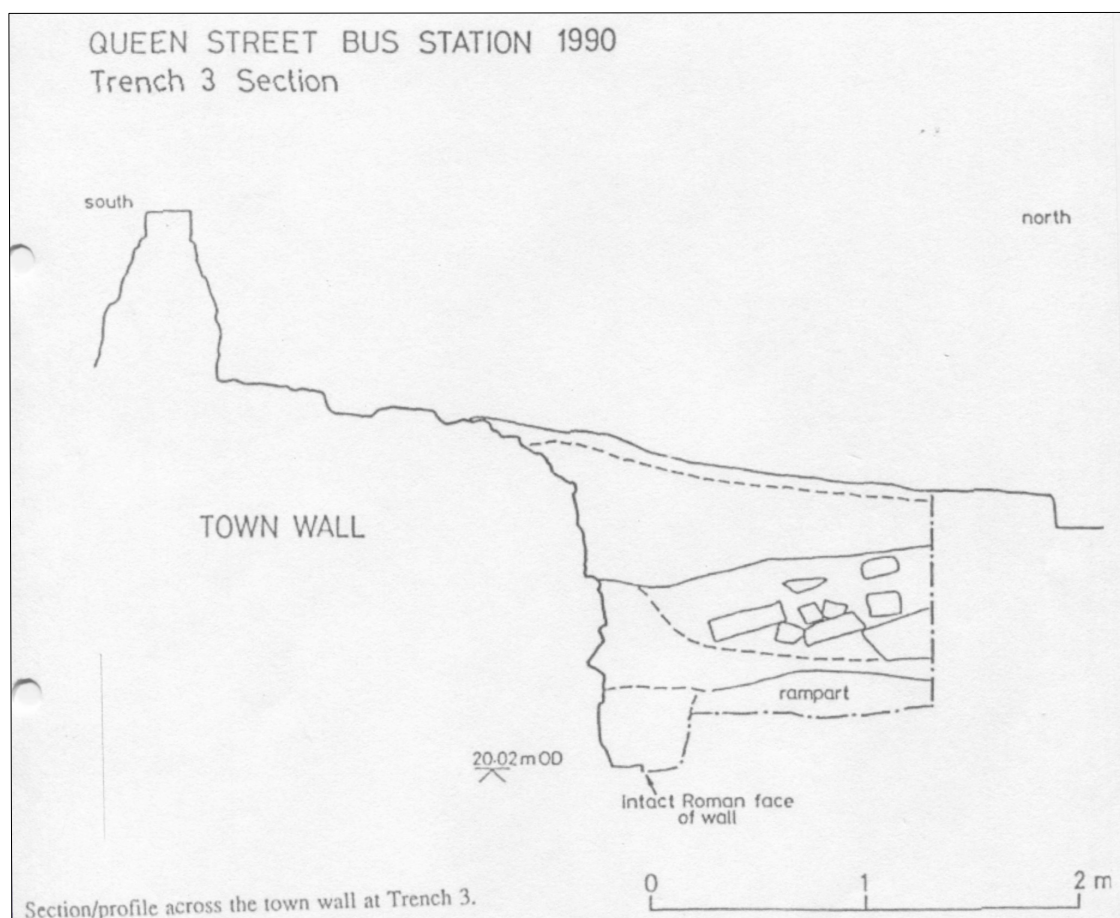
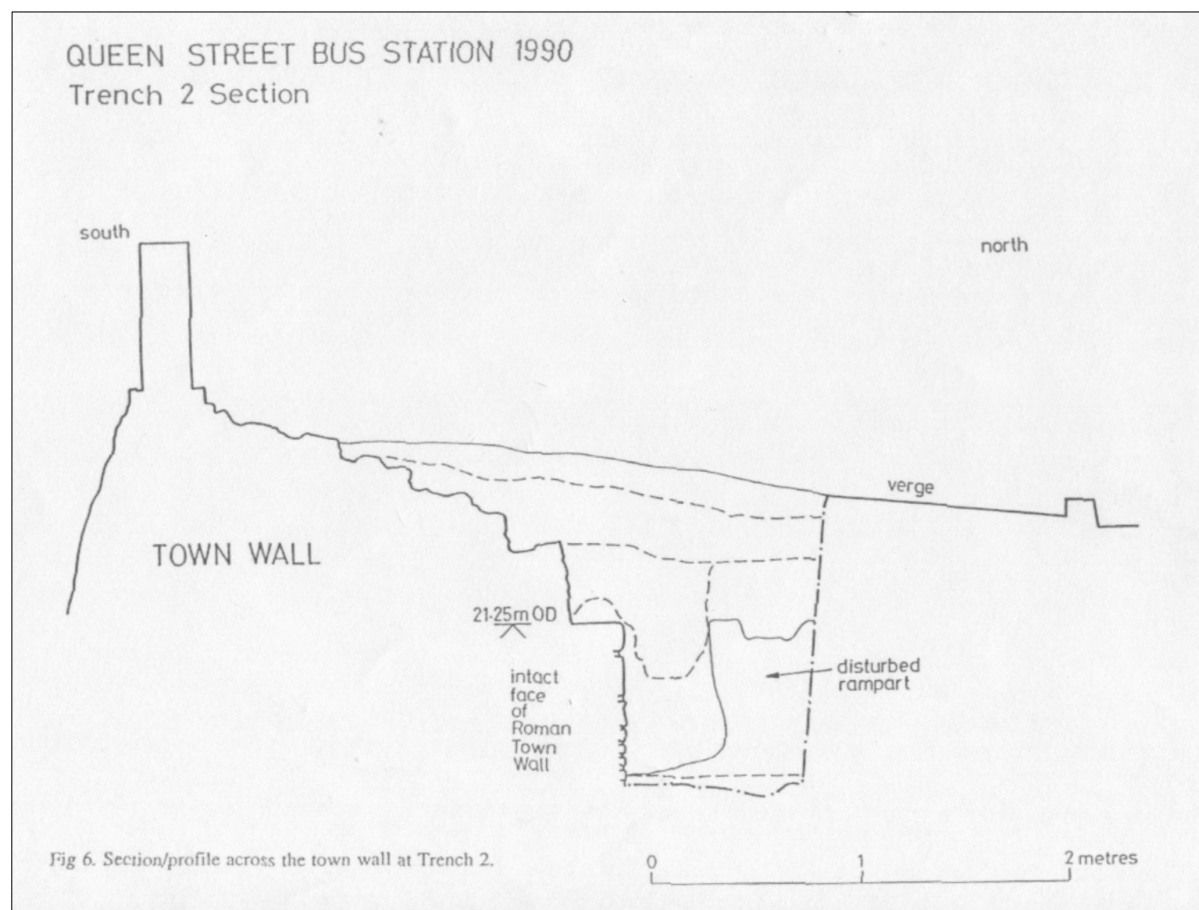


Fig 5 Profiles 2 and 3 across Roman wall, drawn in 1990 (extract from CAT 1990)

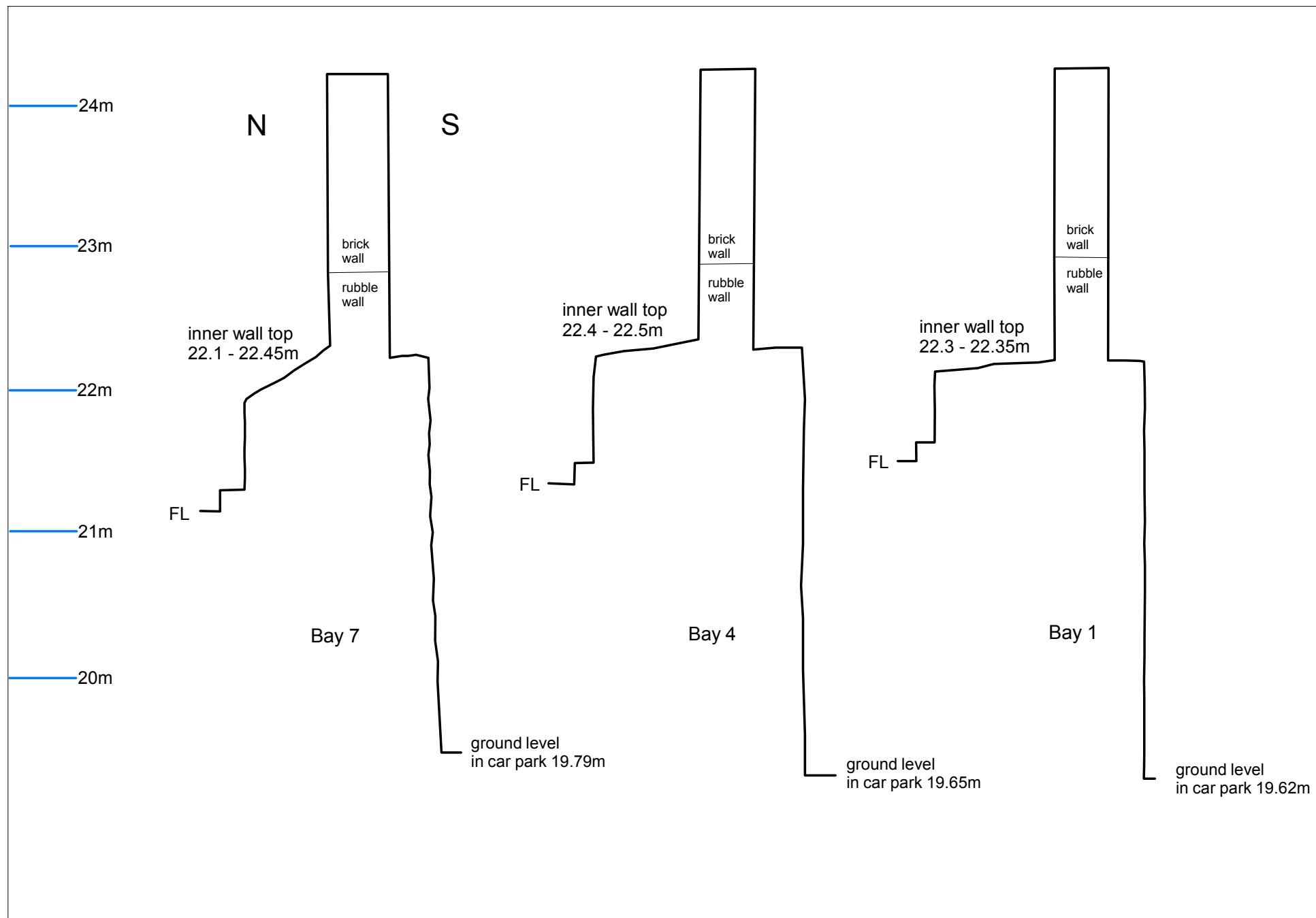
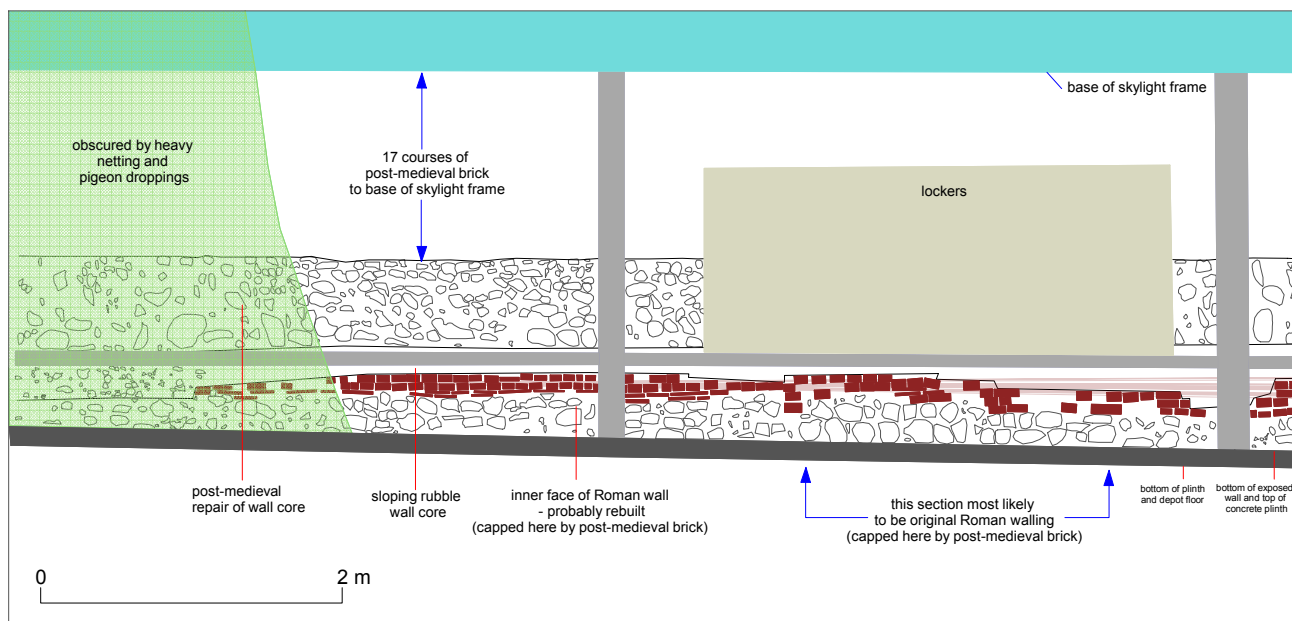
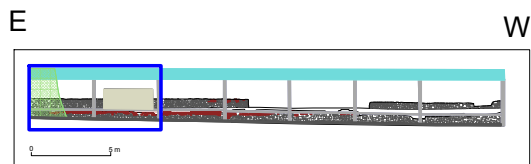


Fig 6 Profile across town wall from Bus Depot to Priory Street car park
Left, Bay 7 (western bay inside Bus Depot), centre, Bay 4 (centre), right, Bay 1 (easternmost bay).

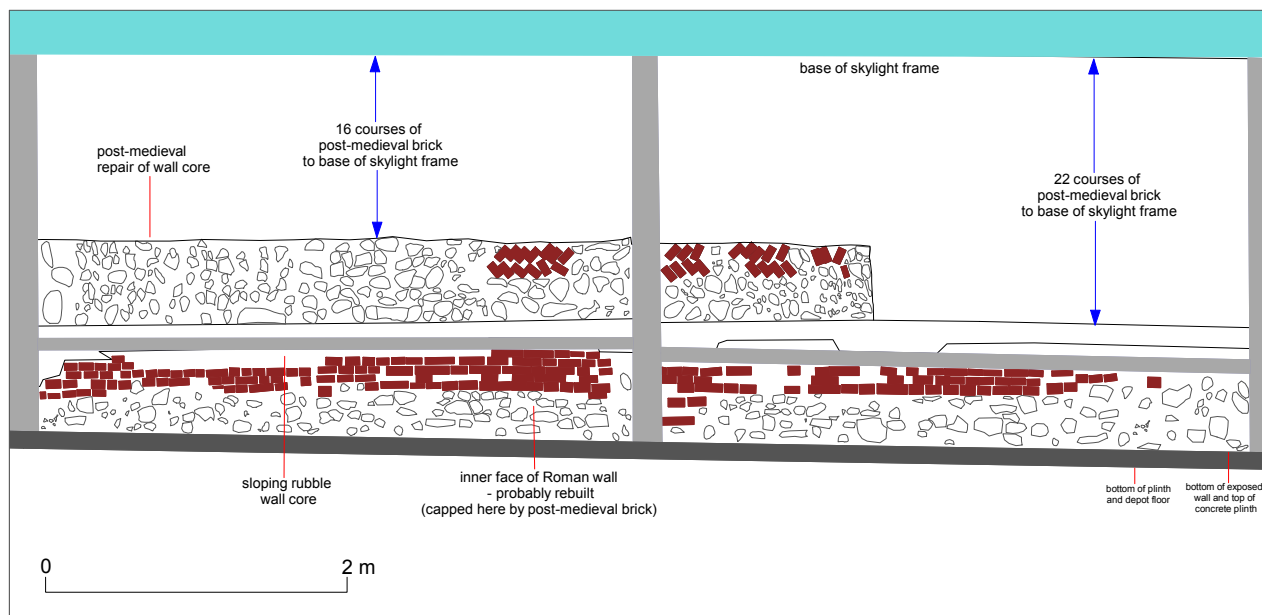
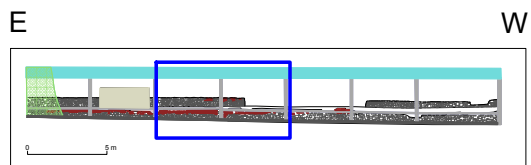


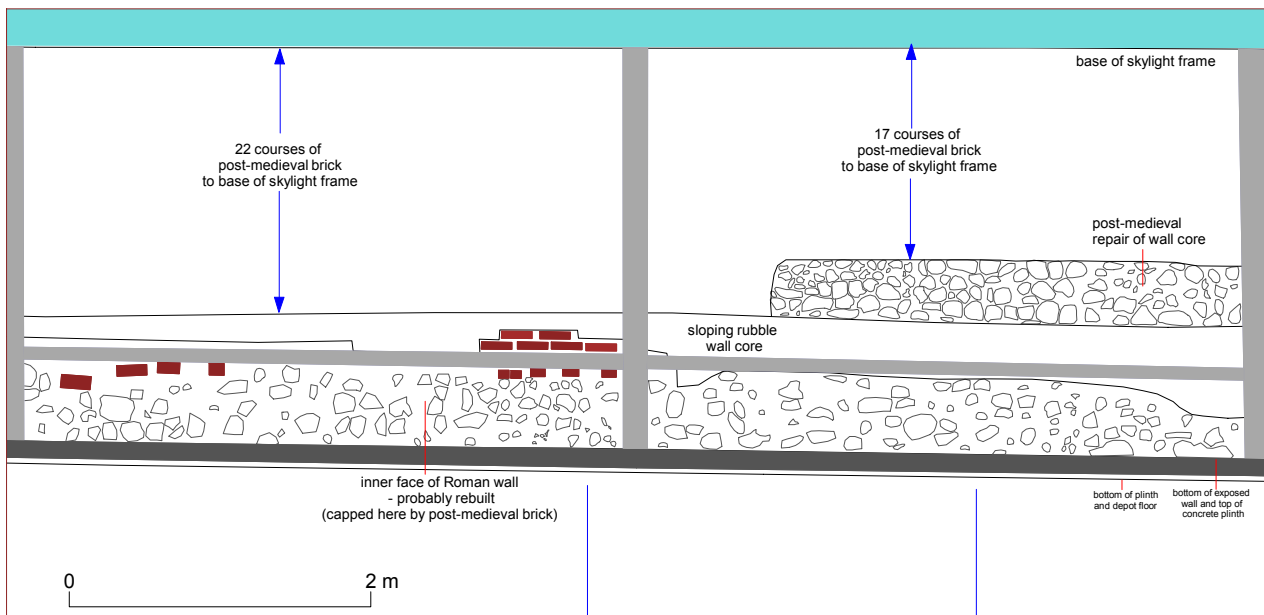
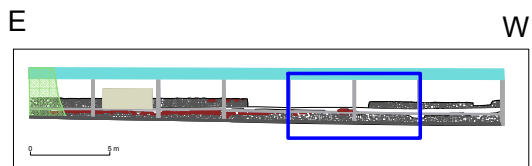
Bay 1 west

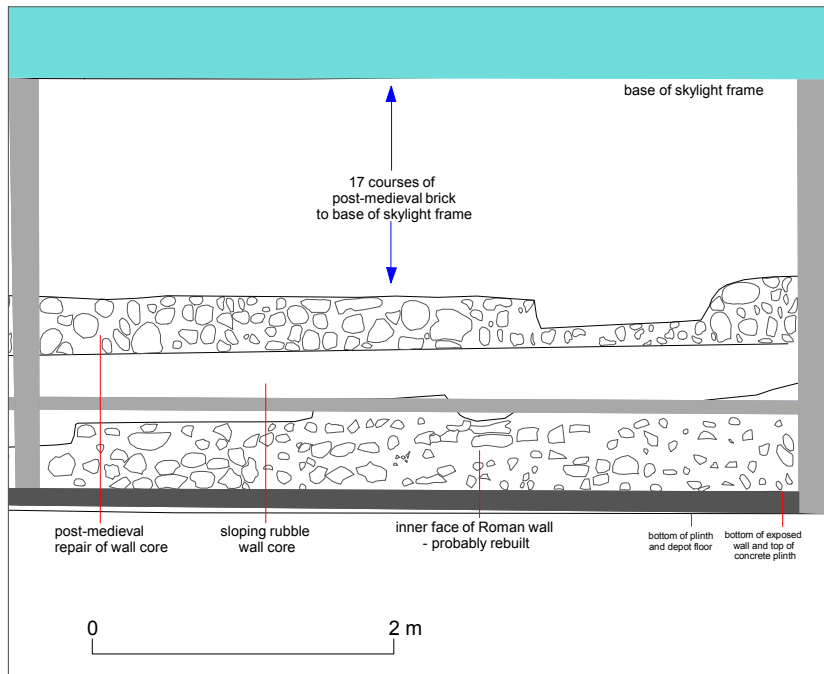
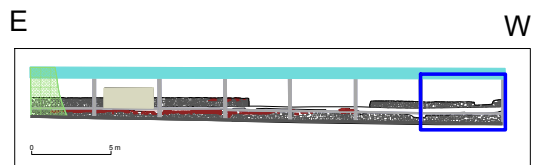


Bay 2: east, central, west

Appendix 1 page 1: Bays 1 and 2 (ie, east end of Bus Depot)
(key at top shows position of this figure - blue box)

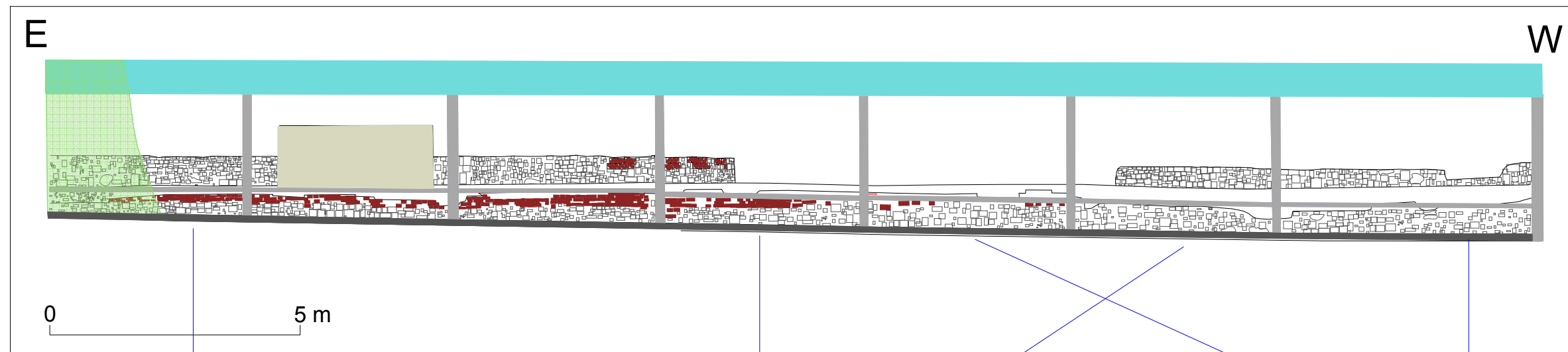






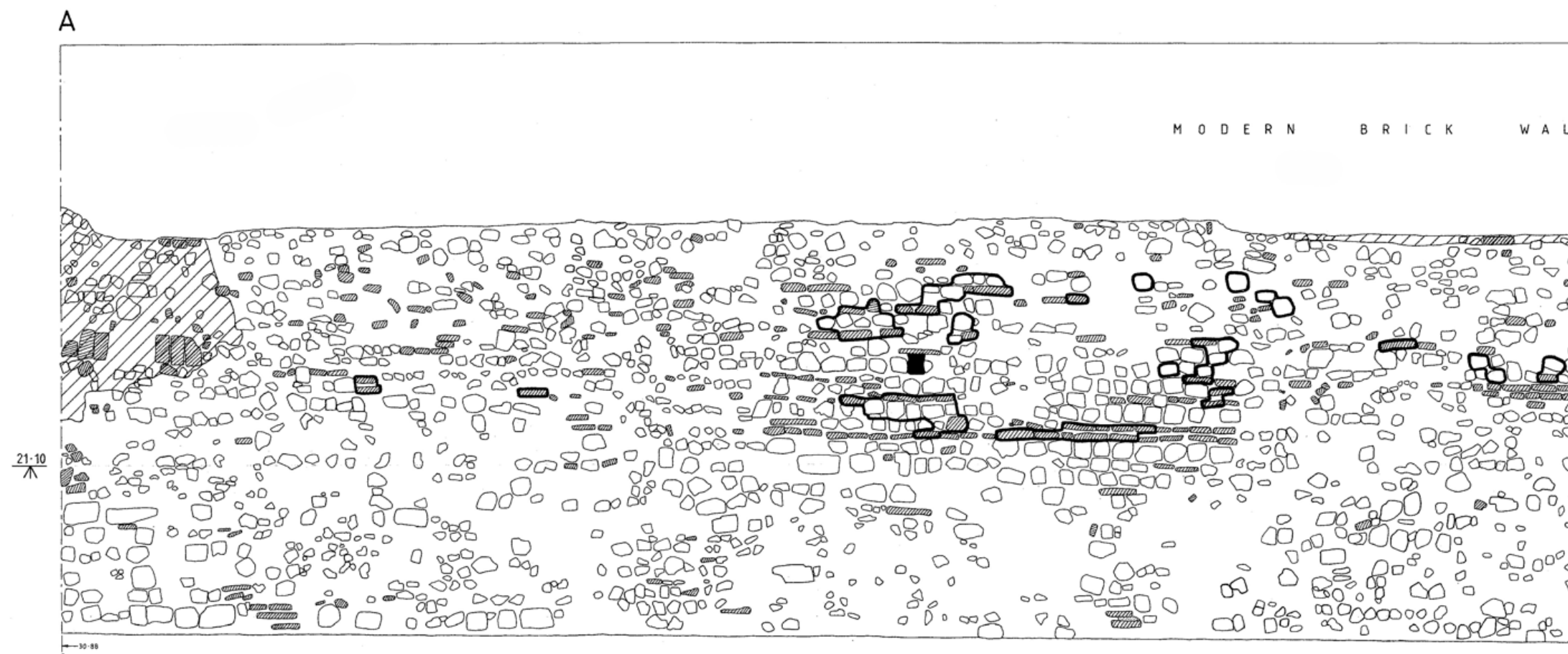


Appendix 2: General photos of Bays 1, 4, 5, 6, 7 (2, 3, most of 4 obscured by screens)



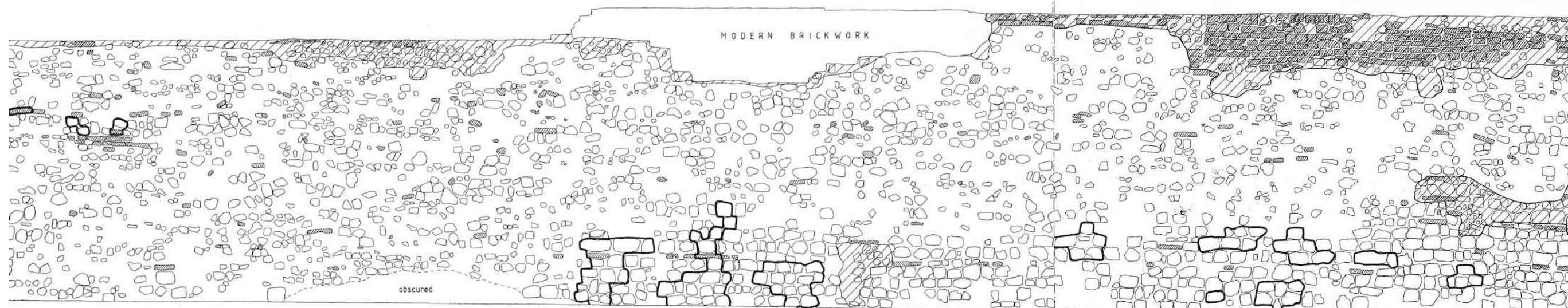
Appendix 2: General photos of Bays 1, 4, 5, 6, 7 (2, 3, most of 4 obscured by screens)

Appendix 3: Priory Street Car Park, Colchester. South face of Roman town wall. 1988 survey drawings and photos
(from west to east)



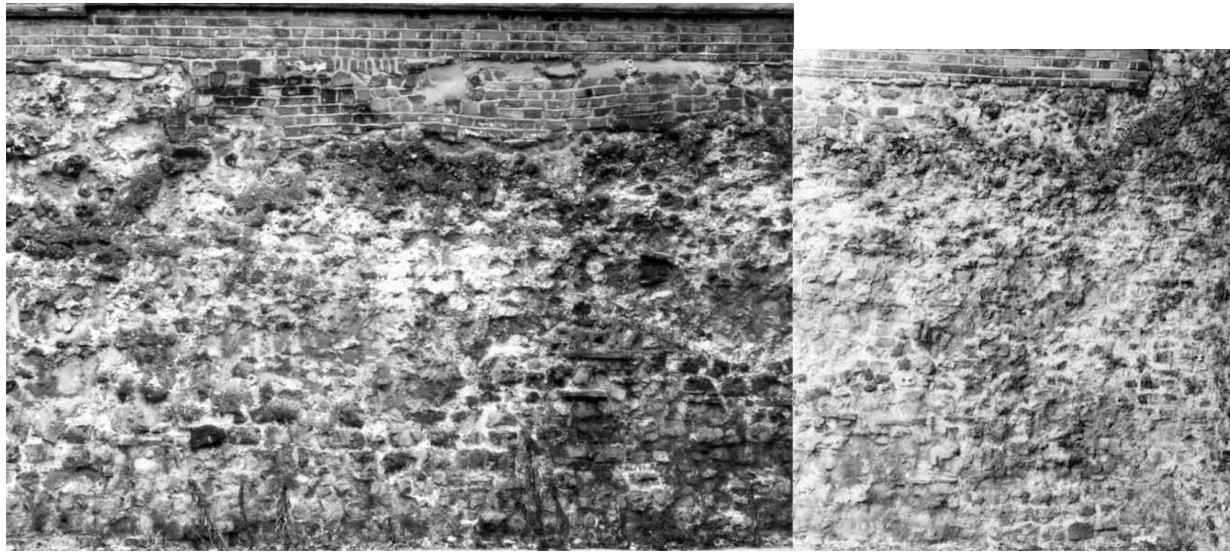
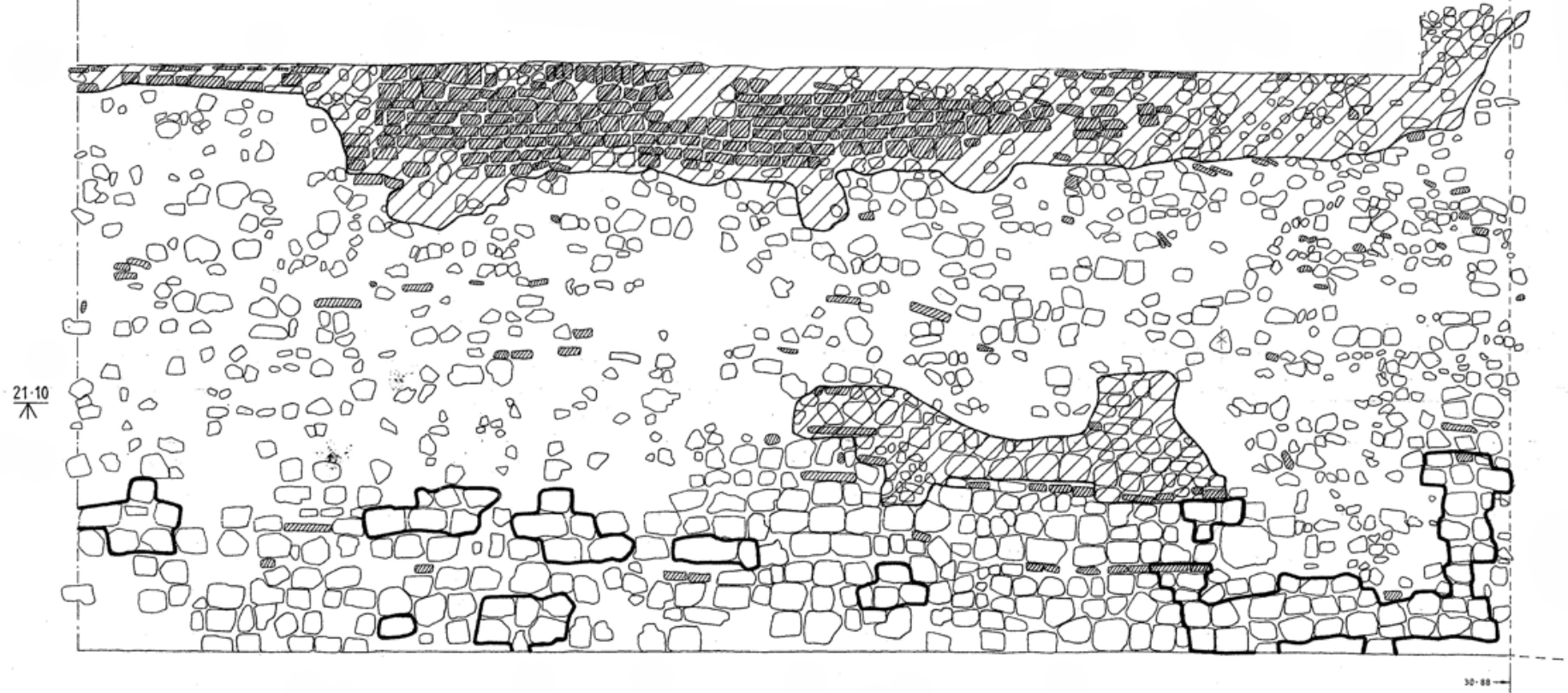
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M O D E R N B R I C K W A L L

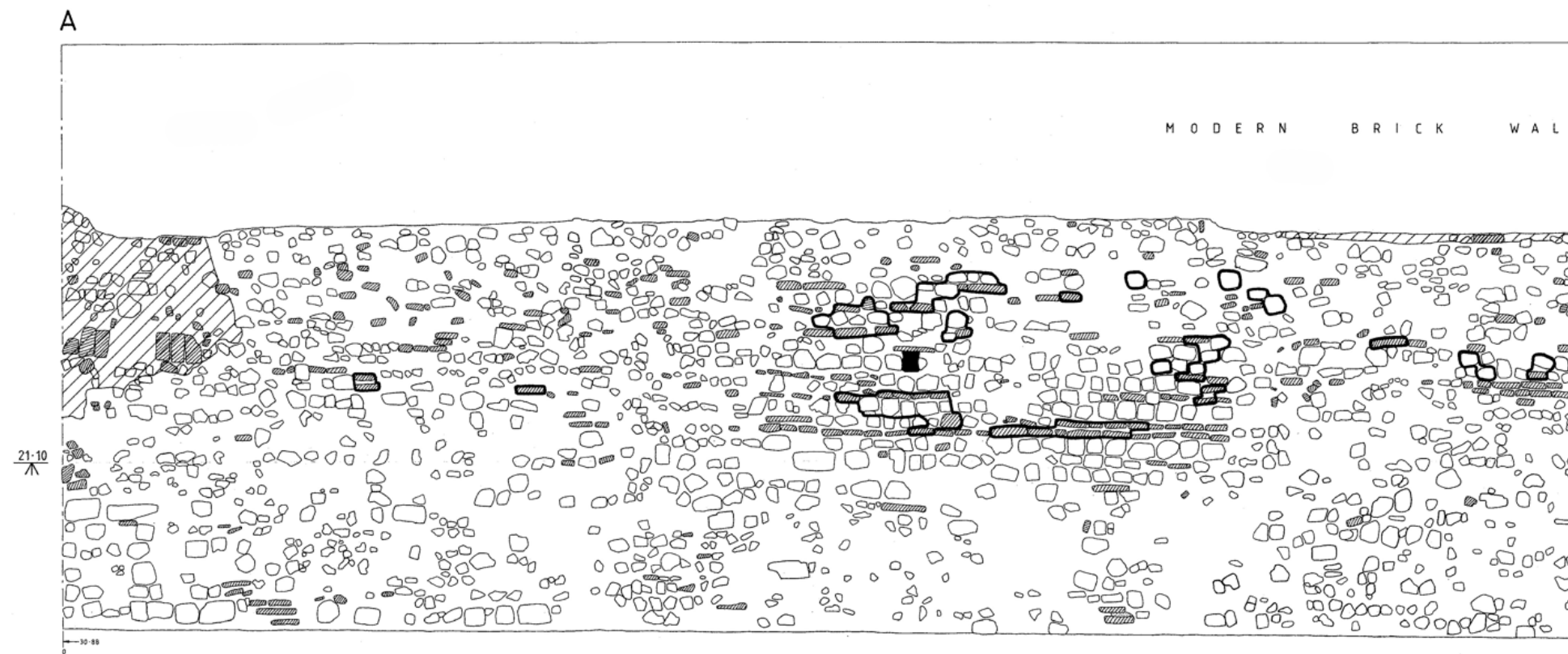


B

M O D E R N B R I C K W A L L

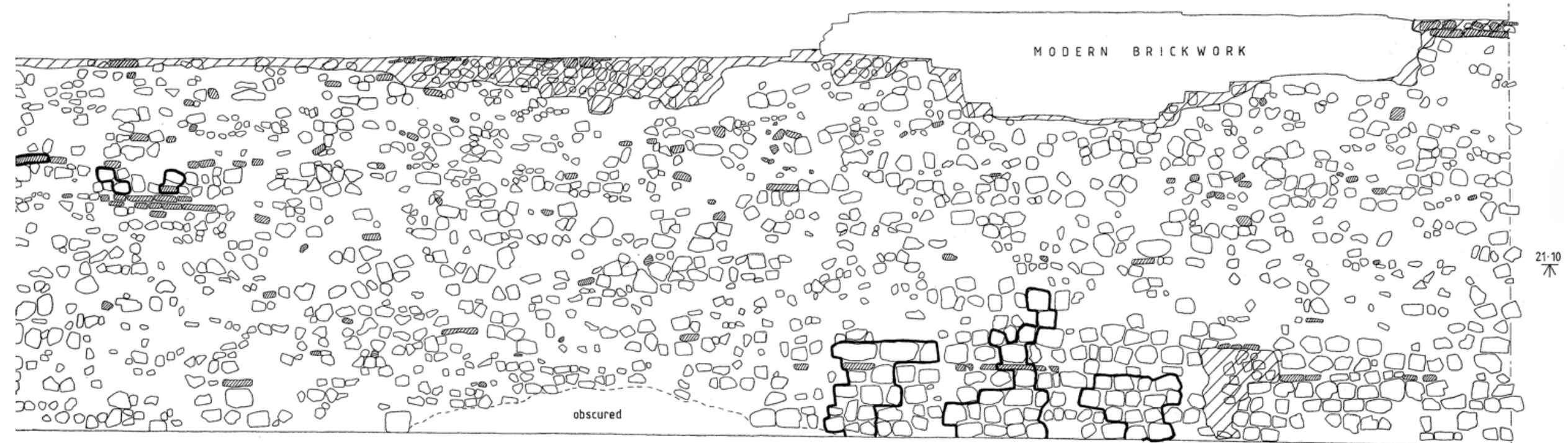


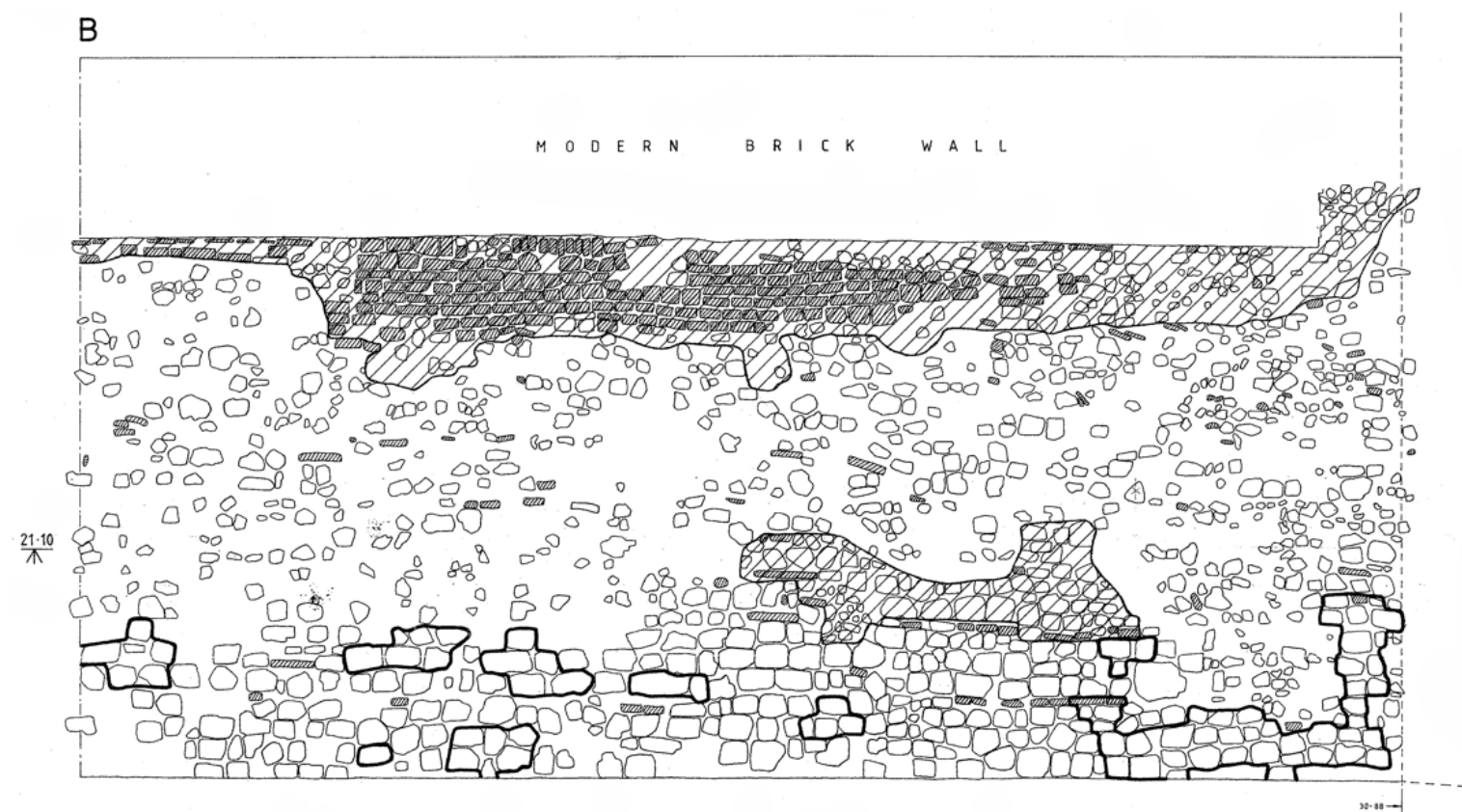
Appendix 4: Priory Street Car Park, Colchester. South face of Roman town wall. 1988 survey drawings, 2016 photos



B

R I C K W A L L





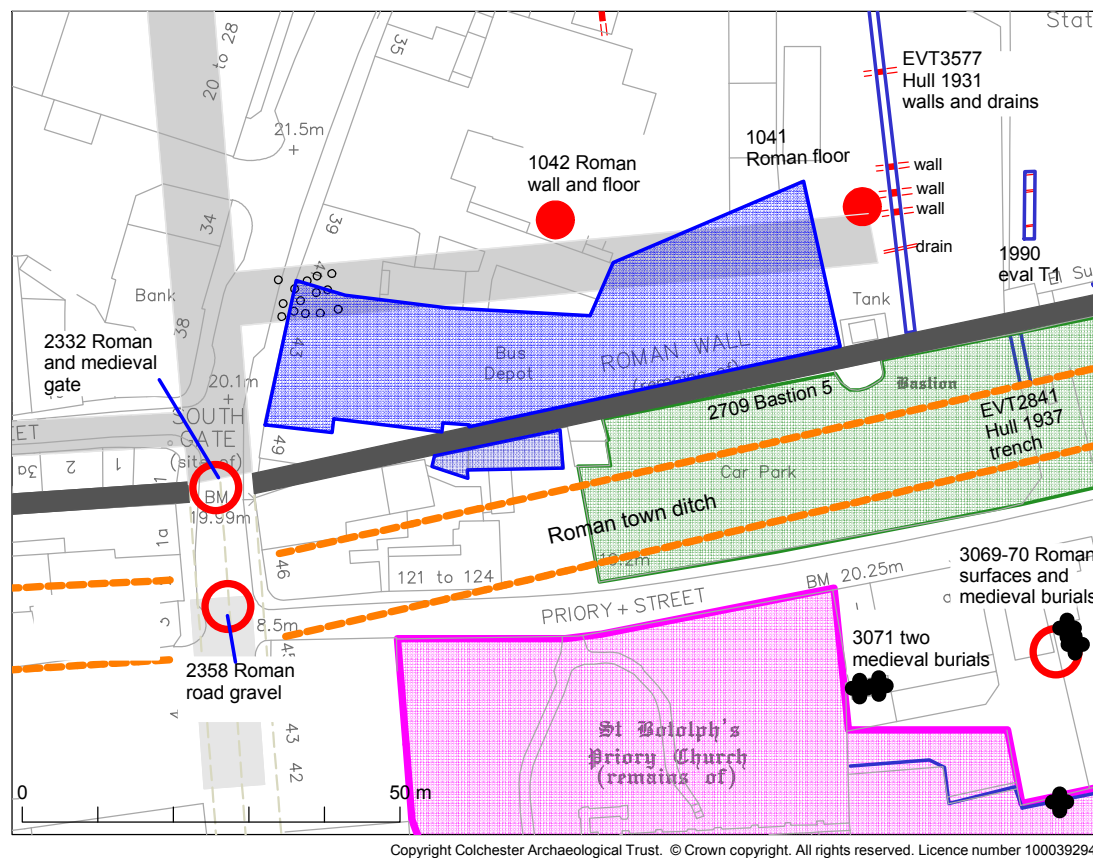


Fig 1 Archaeological discoveries around Bus Depot (light blue shade). The Roman town wall runs along its southern boundary. (Roman streets shown in grey tone). To the north are the buildings of the Roman town. South of the wall is the Roman town ditch (orange), and the Scheduled St Botolph's Priory with associated burials. Priory St Car Park shown in green tone.